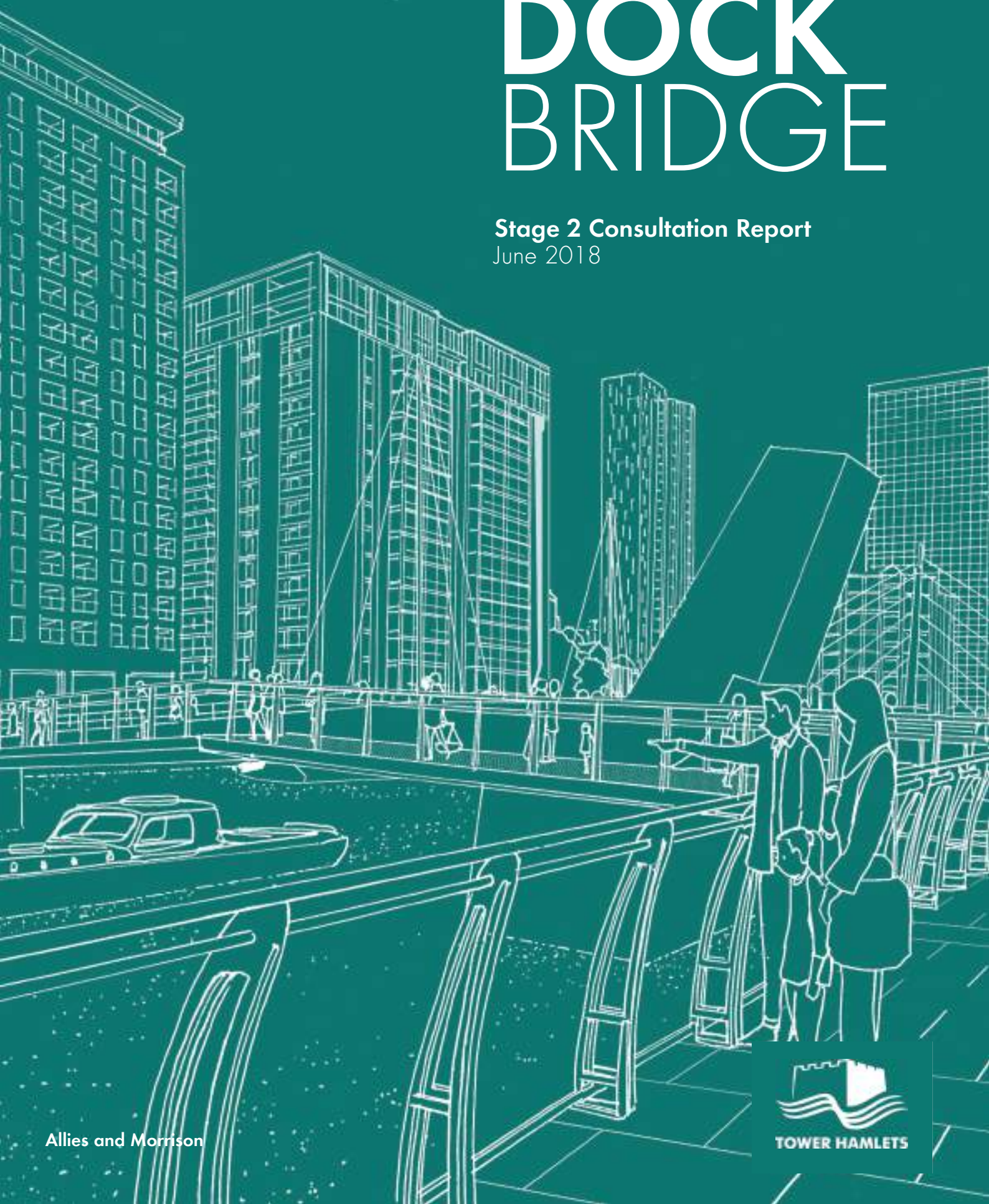


SOUTH DOCK BRIDGE

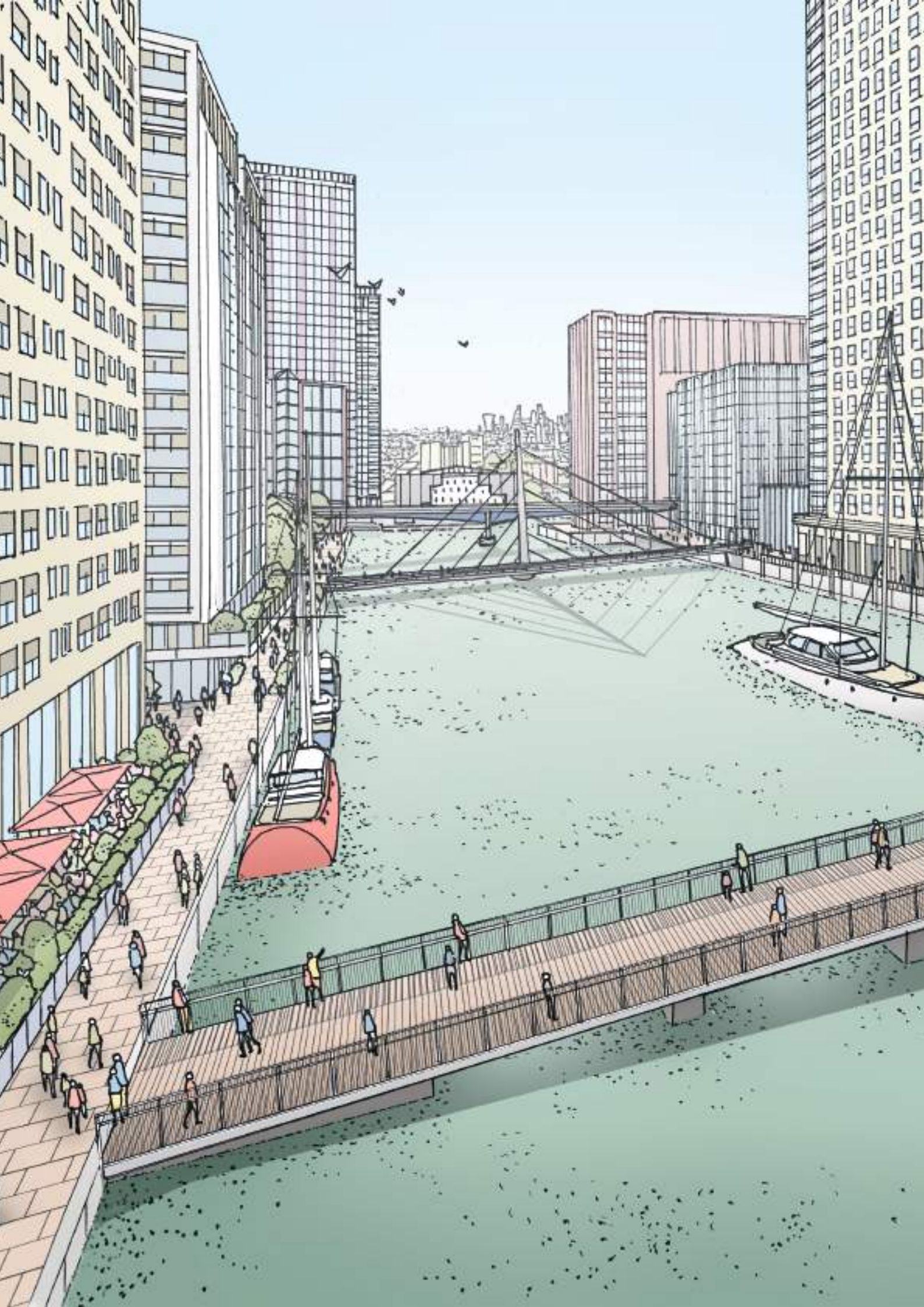
Stage 2 Consultation Report
June 2018

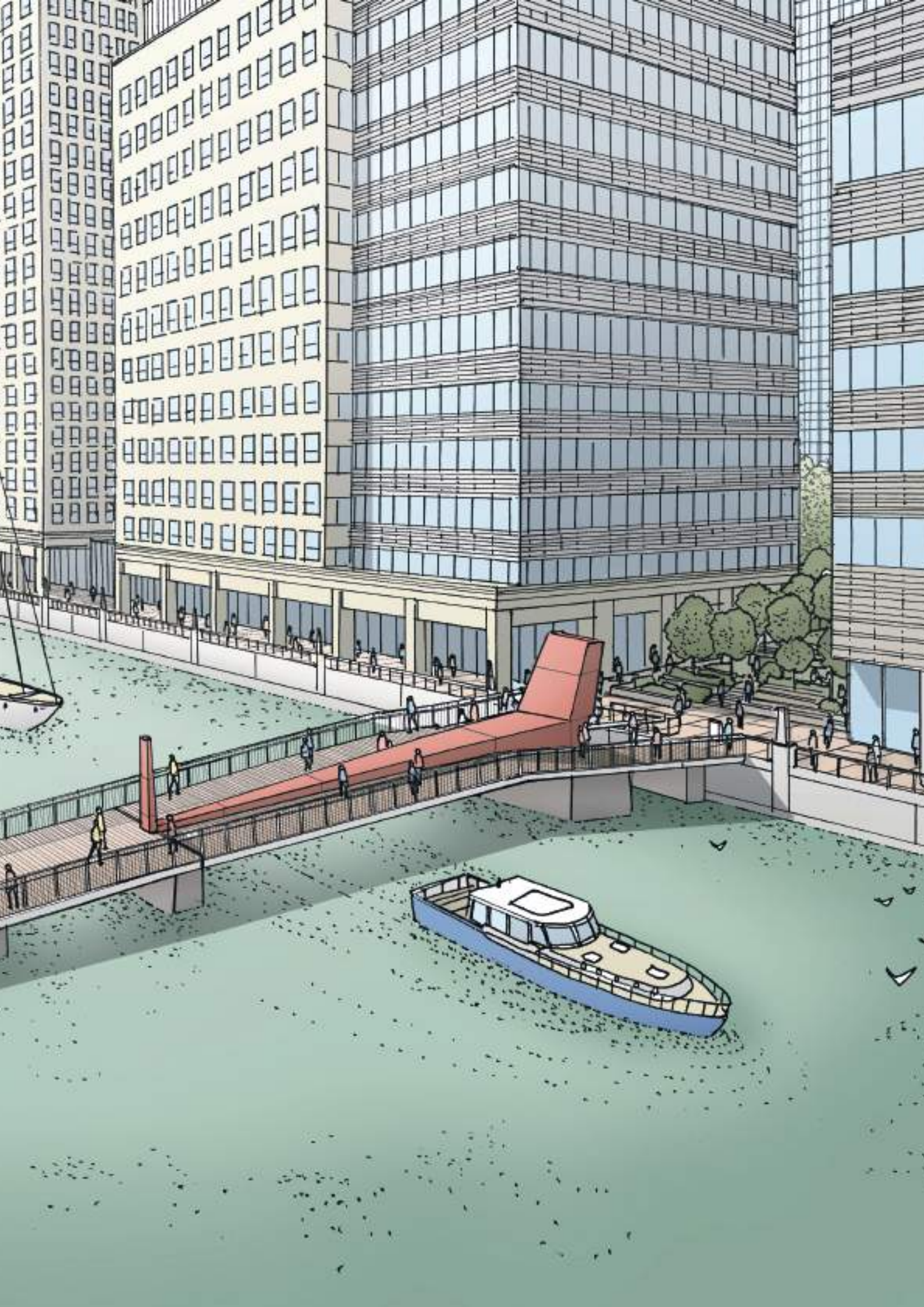


CONTENTS

	EXECUTIVE SUMMARY	6
1	ABOUT THE PROPOSAL	12
2	PRINCIPLES OF ENGAGEMENT	22
3	ABOUT THE CONSULTATION	26
4	ABOUT THE RESPONDENTS	36
5	CONSULTATION RESPONSES REVIEW	42
6	SUMMARY OF KEY MESSAGES	70
	APPENDICES	76







EXECUTIVE SUMMARY

This consultation report has been produced by Allies and Morrison Urban Practitioners on behalf of the London Borough of Tower Hamlets (LBTH).

The report explains the current proposals for the South Dock Bridge, and the purpose, processes, responses and outcomes of the six week consultation period undertaken between Monday 12th February and Friday 23rd March 2018.

This report aims to present and summarise the feedback received, and the implications of this feedback for the design of South Dock Bridge.

We received 161 responses to the public consultation, of which 96 per cent supported the concept design in principle. Summaries of the public's responses can be found in Section 5.

We also received 11 written responses from stakeholders and local residents, giving 172 total responses. Summaries of the written responses can be found in Section 5.11.

In addition to the written responses, we have also received feedback from the consultation events and other meetings with stakeholders.

SUMMARY OF ISSUES RAISED DURING CONSULTATION

Overall, there is a consensus of support for the bridge. A number of key issues were raised during the consultation process, and can be used to inform the brief for the next stage of the project. These key issues are identified below:

MOVEMENT AND ACCESS

Combined pedestrian and cycle use

This was the greatest issue raised during consultation, and prompted a range of responses. Some supported the segregation of cyclists and

pedestrians and others stated that cyclists should not be able to use the bridge at all. There were conflicting views on whether the cycle route would be used by cyclists as a strategic route or not.

The majority of respondents said they would use the bridge on foot. The width of the bridge was the second highest priority and access by bicycle was a 'middling' priority.

This issue should be considered in detail at the next design stage, with Transport for London (TfL)'s strategic objectives; stakeholders' concerns; and public consultation feedback all taken into account. Further stakeholder discussions will be required, along with review of the predicted use levels and the wider movement network.

Approaches from north and south

Respondents stressed that the approaches from the north and south should be spacious and free of obstacles. The approach from the north may require a reworking of the current landscaping, to provide a fully accessible approach that can accommodate flows

The approach from the south was thought to present a challenge in terms of pedestrian/cycle movement flows, South Quay Plaza play areas, and Discovery Dock vehicular access.

These should be reviewed in design terms and in collaboration with key stakeholders at the next design stage.

Wider movement issues

Respondents flagged that cycle and pedestrian routes should integrate well with the wider network. Conflicting opinions were provided on where cycle routes should run south through. This should be explored at the next stage, in conjunction with the balance of cycle and pedestrian use of the bridge.

Seating on the bridge

A small majority of feedback from respondents stated that they do not think seating on the bridge would add value and would likely hinder the movement of pedestrians and cyclists. Some respondents suggested seating might be appropriate if it does not remove movement capacity, and this should be explored at the next stage of design.

CONSTRAINTS AND OPERATION

Maintaining dock access for boats

A number of respondents referenced the requirement to maintain access for the dock area for leisure activities. This should be reviewed at the next design stage with key stakeholders to explore the operational issues.

Requests were also made to ensure that larger vessels can access the dock when the bridge mechanism is open.

Floating pontoons to moor boats

The Canal & River Trust (CRT) highlighted that pontoons for boats to moor in the dock should be provided on the south side of the dock. This has been reviewed in principle but not in terms of detailed design, and should be explored at the next stage.

The bridge should be swift to open and close

This was raised by respondents and Members as an issue with the existing bridge that should not be repeated on the new crossing.

Maintenance and bridge operation

Early agreement on maintenance and bridge operation was sought by the CRT and raised by Members as a key issue. This should be explored by the LBTH team and TfL in collaboration with CRT and other key stakeholders.

East-west wind conditions

Wind conditions for the bridge, and particularly the potential impact of east-west cross winds, was raised by a number of people in written correspondence. Possible design options for mitigating the impact of east-west winds, should be explored at the next stage of the design.

Bridge foundations

The CRT highlighted that the foundations to the bridge will need to be separate from the dock wall. This was a known constraint for the Stage 2 designs and has been taken into account, and must be retained as a design constraint during the next stage.

DESIGN AND AESTHETICS

Design is a priority

'Architectural design, materials and appearance' was selected as the highest priority for respondents who identified their most important aspects for the bridge on the feedback form.

Members' feedback suggested the design process should recognise that the South Dock Bridge will be a landmark structure.

Simple and elegant approach

Most respondents stated that they would like the bridge to be elegant, unobtrusive and contemporary, with a 'neutral finish'. This was reported in the material choices stated in the feedback form and in the reasons provided for the bridge preference. Comments should be used to inform the brief for the next stage of design.

Draw on industrial heritage

Members and some respondents suggested that the area's industrial heritage, dock cranes and working class roots should inform the bridge design. It was suggested that the design team engage with the Friends of Island History Trust on local history, to inform the design development.

Quiet bridge surface

A high priority for respondents is a quiet bridge surface, with references made to the existing bridge failing in this regard. This was also raised in additional comments and written correspondence.

SAFETY AND SECURITY

The bridge should be well lit in the evening

A few respondents raised their desire for the bridge and its immediate surrounds to be well lit in the evening and on winter afternoons, to ensure that users feel safe and comfortable. A lighting strategy should be included in the next stage of design.

Barriers to prevent inappropriate vehicle use

A few respondents and Members raised concerns regarding delivery mopeds and their illegal use of the bridge. Easy access to the bridge by ramp and inclusivity for all users both scored highly on respondents identified priorities for the bridge. These issues will need to be carefully considered during the detailed design phase.

Unwanted congregating on the bridge

A few respondents raised concerns about the potential for groups of people to congregate on the bridge, which could be intimidating, and for 'pan-handlers' to set up on the bridge. One person raised the issue of this being potentially dangerous if cyclists use the bridge.

PROCESS AND ENGAGEMENT

Co-ordinating with Stakeholder strategies

A number of relevant strategies were raised by stakeholders, which should be considered during the next design stage of the project. These include but are not limited to:

1. The South Quay Plaza Construction Management Plan
2. Canary Wharf Group (CWG)'s emerging Water Space Strategy

3. CRT's Code of Practice for Works Affecting the Canal & River Trust

Naming the bridge

It was suggested by the Accessible Transport Forum that the bridge could be named after a notable local person – such as Jack Dash. The group offered to provide a list of suggested names. It was also suggested that LBTH could work with local schools on naming the bridge

Speak with additional stakeholders

The Sea Scouts were identified as an additional stakeholder during consultation and discussions with the group will be ongoing. A review will be undertaken of other potential stakeholders.

Advertising consultation

One person suggested that future consultation events could be advertised using the leaflet stands in Canary Wharf shopping mall.

Compare width to other well known bridges

In discussing the bridge width with consultees at events, it was agreed that it would be useful to provide information on the widths of other well known bridges in London (such as the existing South Dock bridge or the Millennium Bridge) so that a tangible comparison can be made.

NEXT STEPS

The results from the public and stakeholder consultation set out in this report will contribute to the brief for the next stage of the design process. This will enable feedback to directly inform the evolution of the design as a planning application is prepared.

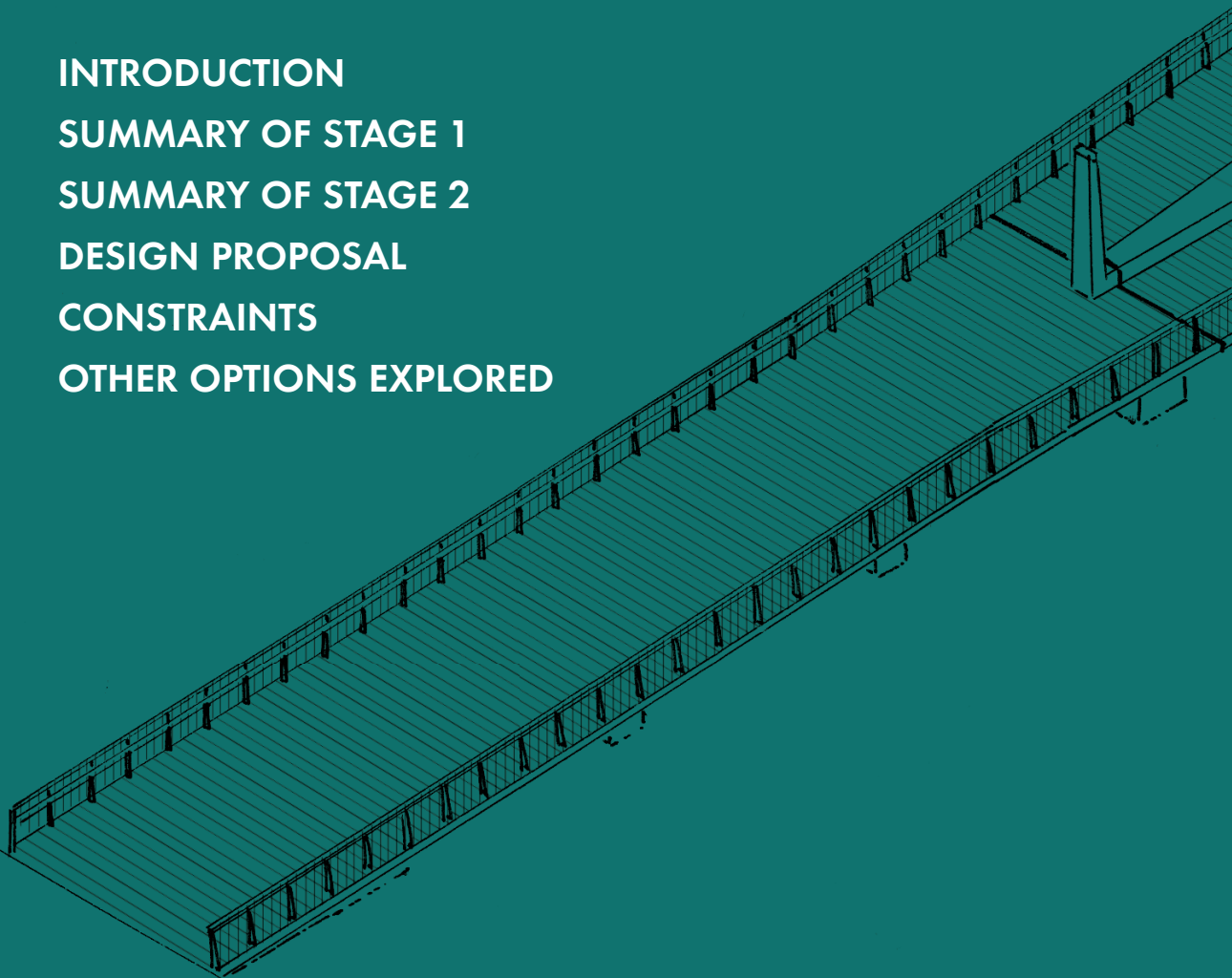
Document abbreviations

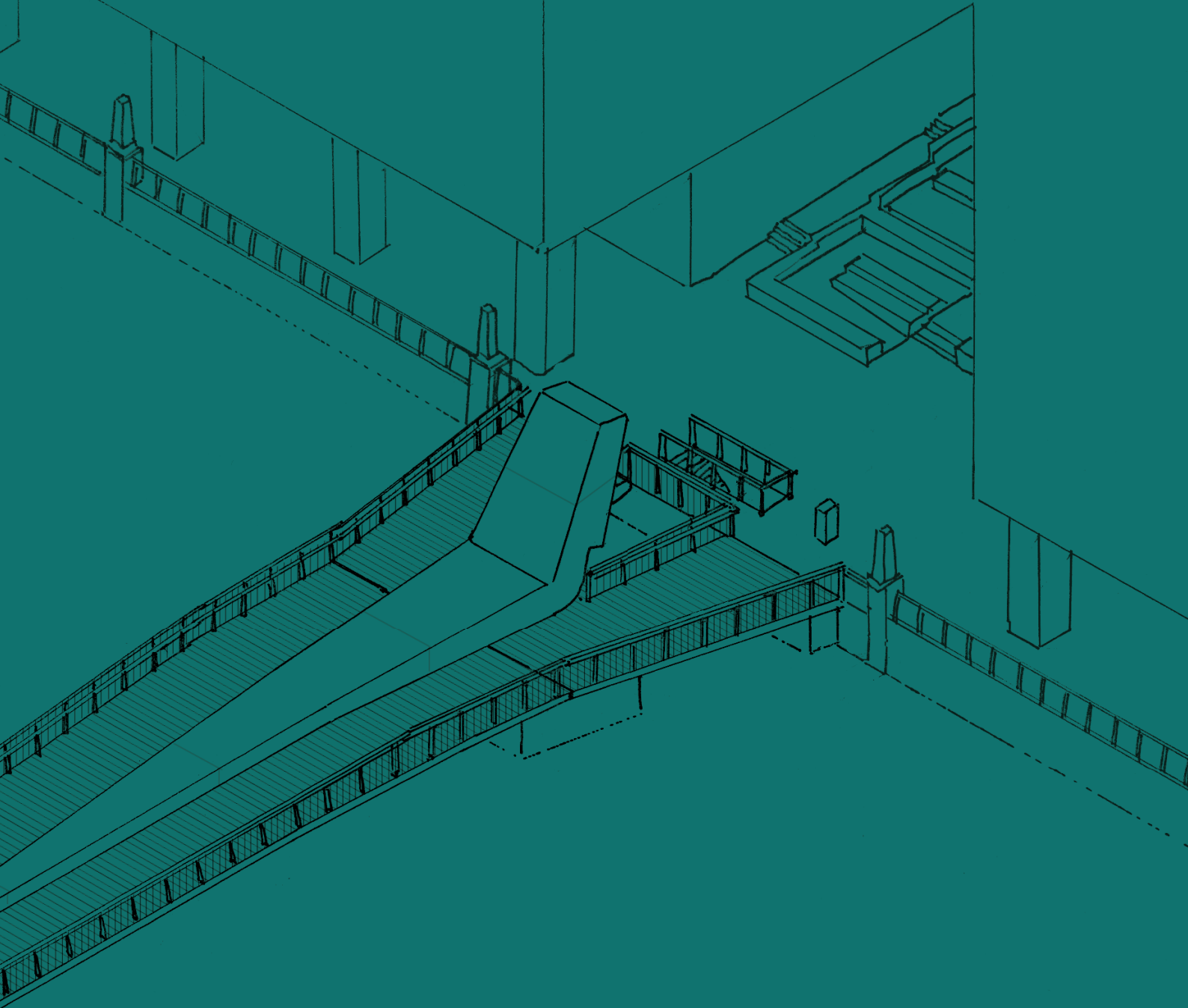
The acronyms below have been employed throughout this consultation report and have been noted here for reference.

- LBTH** = London Borough of Tower Hamlets
- CRT** = Canal & River Trust
- DLR** = Docklands Light Railway
- TfL** = Transport for London
- CWG** = Canary Wharf Group

1 ABOUT THE PROPOSALS

- 1.1 INTRODUCTION
- 1.2 SUMMARY OF STAGE 1
- 1.3 SUMMARY OF STAGE 2
- 1.4 DESIGN PROPOSAL
- 1.5 CONSTRAINTS
- 1.6 OTHER OPTIONS EXPLORED





TOWER HAMLETS

SOUTH DOCK BRIDGE

Consultation Report
May 2018

1.1 INTRODUCTION

South Dock is one of two surviving docks in the former West India Dock, which lies between Canary Wharf and South Quay. There are two existing bridges over the dock: the DLR south of Heron Quays station and the Wilkinson Eyre pedestrian bridge south of the West Wintergarden.

Of the 37,000 people who move between the north and south sides of South Dock each day, around 27,000 use the Wilkinson Eyre bridge. This makes the bridge the second busiest pedestrian only bridge in London, by pedestrian demand, only being surpassed by the Golden Jubilee footbridge across the River Thames in central London.

It is anticipated that demand for crossing South Dock could potentially increase to 85,000 people by 2031 as a result of:

1. Planned developments in the area
2. People seeking to access Canary Wharf Jubilee Line and Elizabeth Line stations
3. People using South Dock DLR station and crossing South Dock on foot in preference to Canary Wharf DLR station.

It has therefore been proposed that a new bridge should be developed to help alleviate some of the pressure faced by the existing infrastructure.

The key objectives of South Dock Bridge

1. To link new development on the Isle of Dogs with Canary Wharf and Wood Wharf.
2. To share the load of new crossing demand with the existing bridge.
3. To shorten walking and cycling times to the new Crossrail (Elizabeth Line) station and other public transport links, as well as improving access to jobs, retail and other town centre services at Canary Wharf.
4. To be wheelchair accessible and cater for all potential users.

The council is also considering how the bridge will integrate with the wider walking and cycling network on the Isle of Dogs, which are due to be enhanced in coming years.

1.2 SUMMARY OF STAGE 1

Steer Davies Gleave, Allies and Morrison, and Arcadis delivered the Stage 1 feasibility report for South Dock Bridge in May 2016. This report considered several aspects:

1. Strategic and policy context: the bridge's fit with regional and local policy
2. Existing and future situation and movements: an assessment of the existing land use and movement patterns in and around South Dock, as well as a summary of planned future developments.
3. Stakeholder consultation: engagement with key stakeholders including the CRT and adjacent land owners
4. Bridge type options: an assessment of different bridge types, e.g. swing, retracting, single-leaf bascule.
5. Bridge alignment options: consideration of six possible north/south alignments across South Dock, as well as the potential for combinations of alignments.
6. Demand forecasts: modelling of future pedestrian and cyclist demand to inform recommended bridge widths.
7. Benefit: cost ratio: a calculation of indicative costs and anticipated benefits of the bridge.

The Stage 1 report concluded with the following recommendations:

8. A single-leaf bascule bridge is the most appropriate bridge type.
9. A combination of two bridges should be constructed in a phased manner.
 - A new bridge on the line of Upper Bank Street at Alignment 5
 - A new bridge to replace the Wilkinson Eyre Bridge on Alignment 3.

1.3 SUMMARY OF STAGE 2

The same design team was remobilised in October 2017 to progress the concept design of the bridge on the line of Upper Bank Street.

The design team undertook the following activities between November 2017 and March 2018:

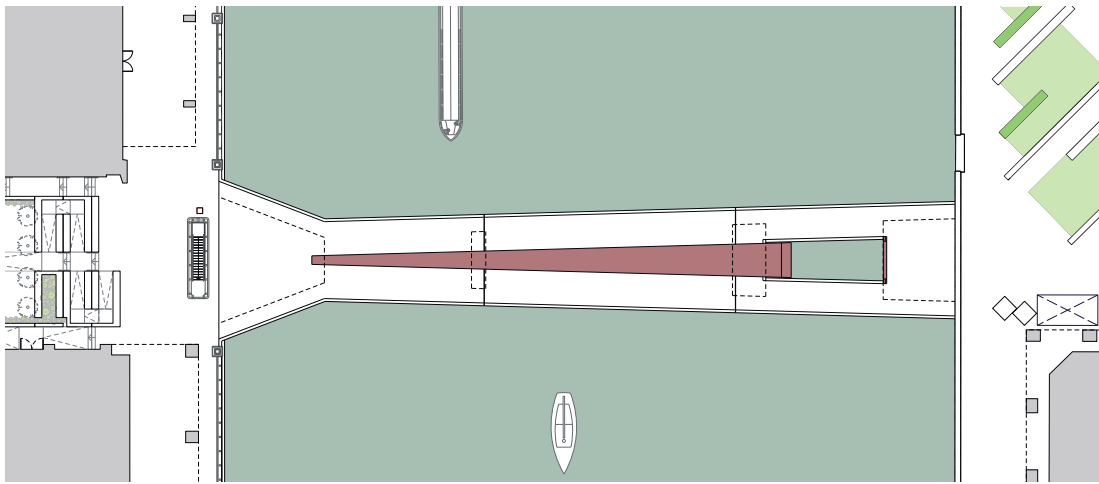
1. Demand forecast update
2. Bridge design development
3. Stakeholder engagement
4. Public consultation
5. Bridge approach landscapes
6. Engineering and construction
7. Programme and next steps

The purpose of the Stage 2 consultation process is to establish public and stakeholder views on the latest proposals for South Dock Bridge.

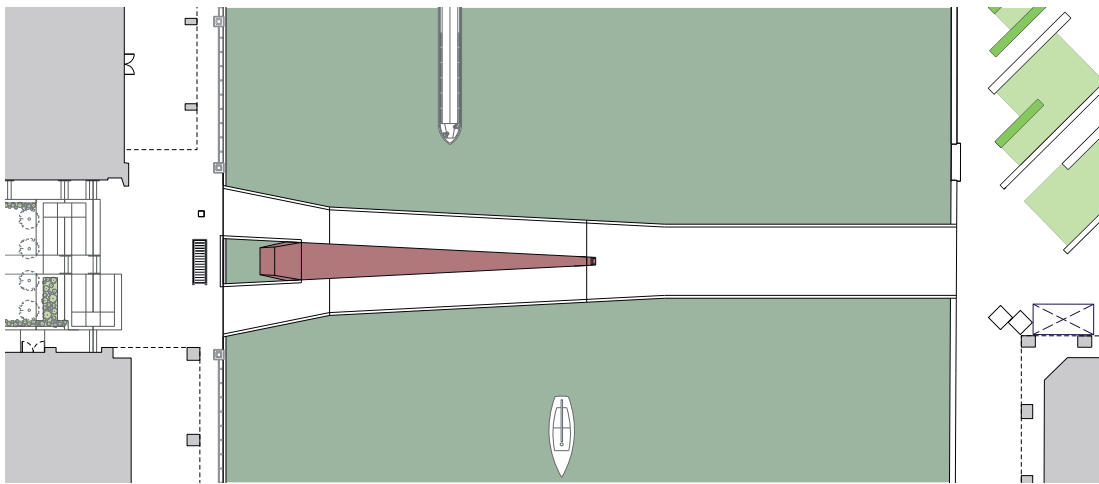
Two initial options, A and B, were developed for the bridge. The start date for the Public Consultation required a bridge option to be provisionally agreed early in the concept design work stage for the purposes of producing drawings and visualisations to show to the public, whilst design development and stakeholder engagement was ongoing and before it was possible to provide any significant engineering input to the emerging concept options.

Option B was selected since the initial responses from key stakeholders CRT and Berkeley Homes supported the rationale for preferring Option B.

The bridge design concept illustrated for the Public Consultation is described overleaf.



Bridge plan - Option A



Bridge plan - Option B

1.4 DESIGN PROPOSAL

The bridge design concept has a single leaf counterweighted bascule with a fixed pivot set at the northern Herons Quay end. The opening span includes the 15m wide permanent navigation channel. A central spine beam above the deck splits it in two and the deck then separates into two distinct surfaces separated by water, fanning out to provide two routes onto the bridge either side of the retained fire escape stairs on Herons Quay. The counterweight swings down when the bridge is opened to occupy the space between the separated decks.

The bridge deck has a smooth steel underside which tapers at the edge to give a thin elevation, and the deck is supported on lozenge shaped concrete piers that are inset by 1.8m to correspond with the tapered deck edge cantilever. The spacing of the piers on the fixed southern span are arranged to support the thin deck with no additional longitudinal structure.

The proposed bridge overcomes one of the limitations of the existing Wilkinson Eyre bridge by providing waiting areas on the fixed part of the structure whilst the lifting section is open thereby avoiding overcrowding on the quay sides. It is anticipated that automatic drop down barriers stored in a vertical steelwork post which is part of the above deck structure composition would be deployed when the bridge is opened to provide a safe edge for the public.

The bridge parapets are envisaged as open metalwork panels with slender vertical infill balusters to provide the maximum transparency. The top rail of the parapet will be set at 1.4m above the deck as required by the current standards for a cycle bridge. A more generous leaning rail will be provided at a lower level.

The bridge deck surface should be quiet to walk on and a system of recycled polymer decking planks set transversely across the deck is

proposed, fixed to the structural steel deck plate with isolating pads to minimise the noise from footfall, buggies, and wheeled suitcases.

The bridge proposal with an above deck spine beam lends itself to the possibility of integrating seating which may be fixed to the structure. This will be explored further in the next work stage.

Bridge Materials

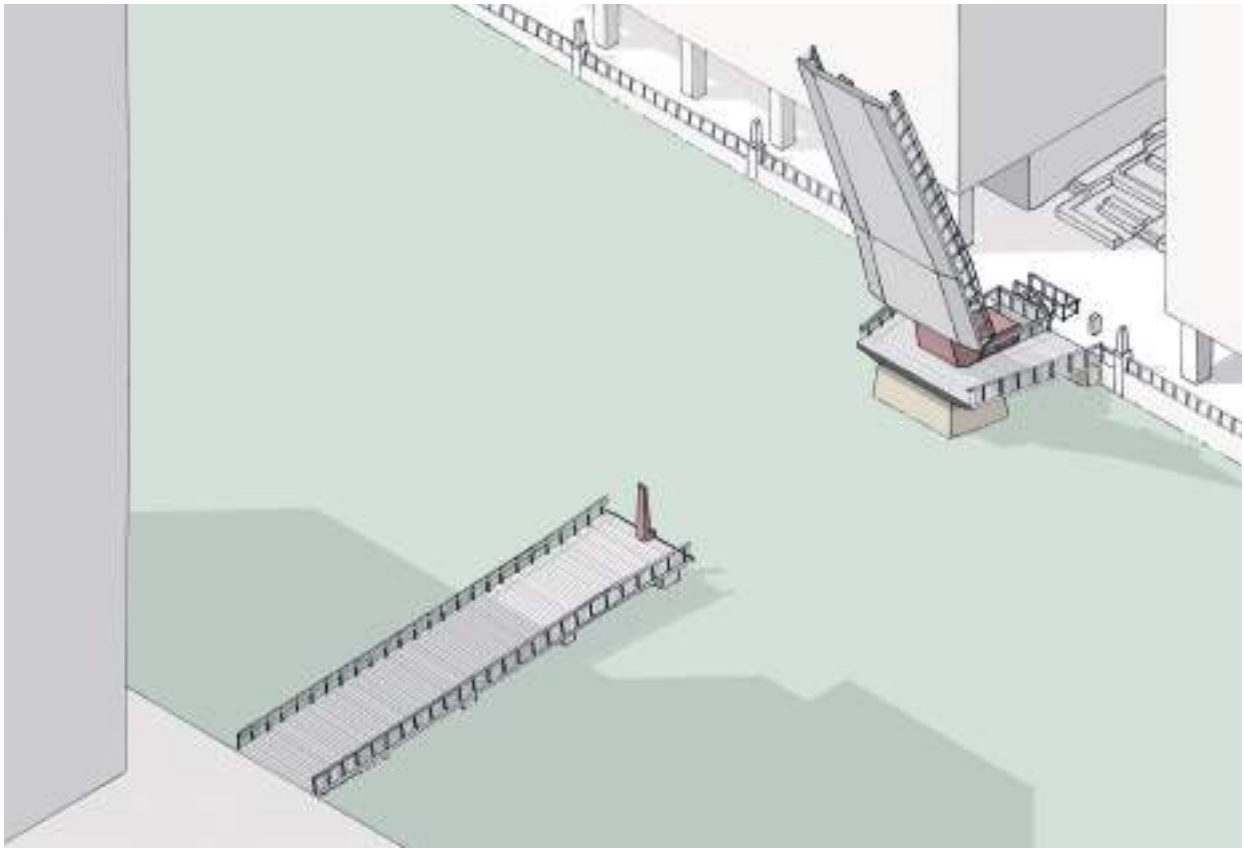
A steel bridge as proposed will generally be painted as part of the steelwork protection, and so the question of colour arises, and what will be appropriate to the design and the surrounding context. This will be explored further in the next work stage.

Examples of opening bridges

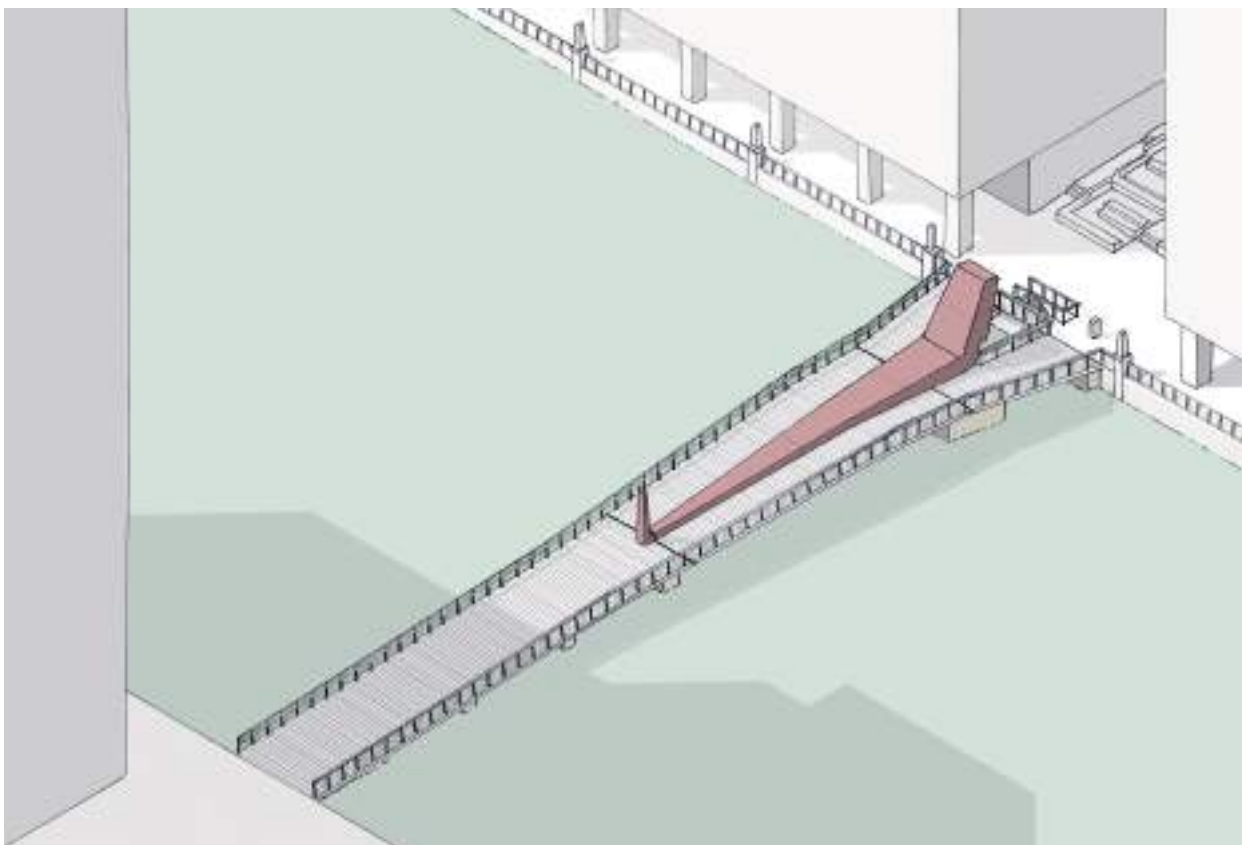
Similar opening pedestrian and cycle bridge precedents have been considered during this work stage, to help illustrate what a built bascule bridge with a fixed pivot may look like.

The public was consulted on the following options:

1. The treatment and surface finish of the steel the bridge will be constructed from
2. The preferred example of similar opening and cycle bridge precedents
3. How valuable places to sit on the bridge, or
4. A central viewing platform would be as an addition to the bridge
5. Which aspect of the bridge they considered to be the most important



Proposed Bridge Open



Proposed Bridge Closed

1.5 CONSTRAINTS

The location of the proposed South Dock Bridge is in a busy urban area and spans an active waterway. These characteristics place a number of constraints on the design options for the bridge, which include:

1. The bank on the north dock is higher than the south dock, and the bridge must overcome this level difference, whilst having comfortable gradients for all users.
2. The approach areas must balance pedestrian flows with the role of these spaces for play and amenity - particularly on the south bank.
3. The bridge must be a minimum of 7.8m wide for pedestrians and cyclists to use comfortably.
4. The pedestrian and cycle paths will not be segregated as this would require a wider approach path than is available.
5. On the north bank, a basement exists with fire escape steps leading to dock level. This access must be retained.

Importantly, the bridge must maintain access to the dock for boats, and key issues relating to this include:

6. The bridge must have a permanent navigable channel 15m wide for smaller boats to pass underneath.
7. The bridge must open to give a 25m wide navigable channel for taller boats.
8. Boat mooring points must remain available on the south dock wall.
9. The opening mechanism must be swift - the existing bridge requires long waits while it opens and closes.
10. Space should be available for pedestrians to wait on the bridge rather than having to wait on the dockside where there is limited space.

These key constraints were identified in the public exhibition, so visitors could understand the technical issues that had informed the concept design.

1.6 OTHER OPTIONS EXPLORED

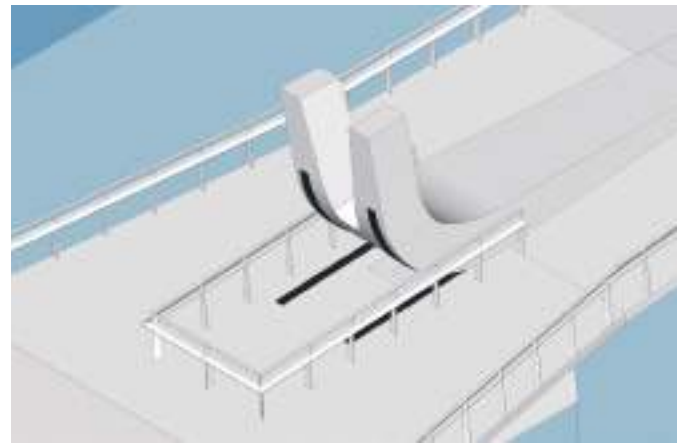
Rolling Spine Bascule Bridge

During this work stage various structural options have been considered in the form of rolling bascule bridges. A rolling bascule differs from a pivoting bascule in that the lifting span is raised by rolling back on a curved segment of what is usually the truss structure. Rolling bascules were extensively used in historic bridges but modern examples can be found. They have an advantage that since they roll out of the way, they more easily achieve the required clear navigation channel for a given span than a pivot bascule which never opens a full 90 degrees. They are efficient in moving but can require more maintenance due to the tracks required for them to roll in.

A spine beam version of a rolling bascule has been considered but this has been discounted as the rolling tracks are set too close together and the structure would be unstable.

Rolling Truss Bascule Bridge

A more conventional rolling bascule with trusses at the deck edges rolling at the northern end will not work in the South Dock as it precludes the fanning out in plan of the deck necessary to negotiate the fire escape stairs on Herons Quay. Reversing the lifting span to roll at the southern end will work, however the overall composition of the bridge crossing comprising fixed spans with a thin deck and a lifting span with a deep truss set above the deck would lack elegance and coherence. Trusses above the deck are a common structural solution but in this case will not allow for a thin cantilevered deck edge with a lightweight and transparent parapet. For these reasons this option was also discounted.



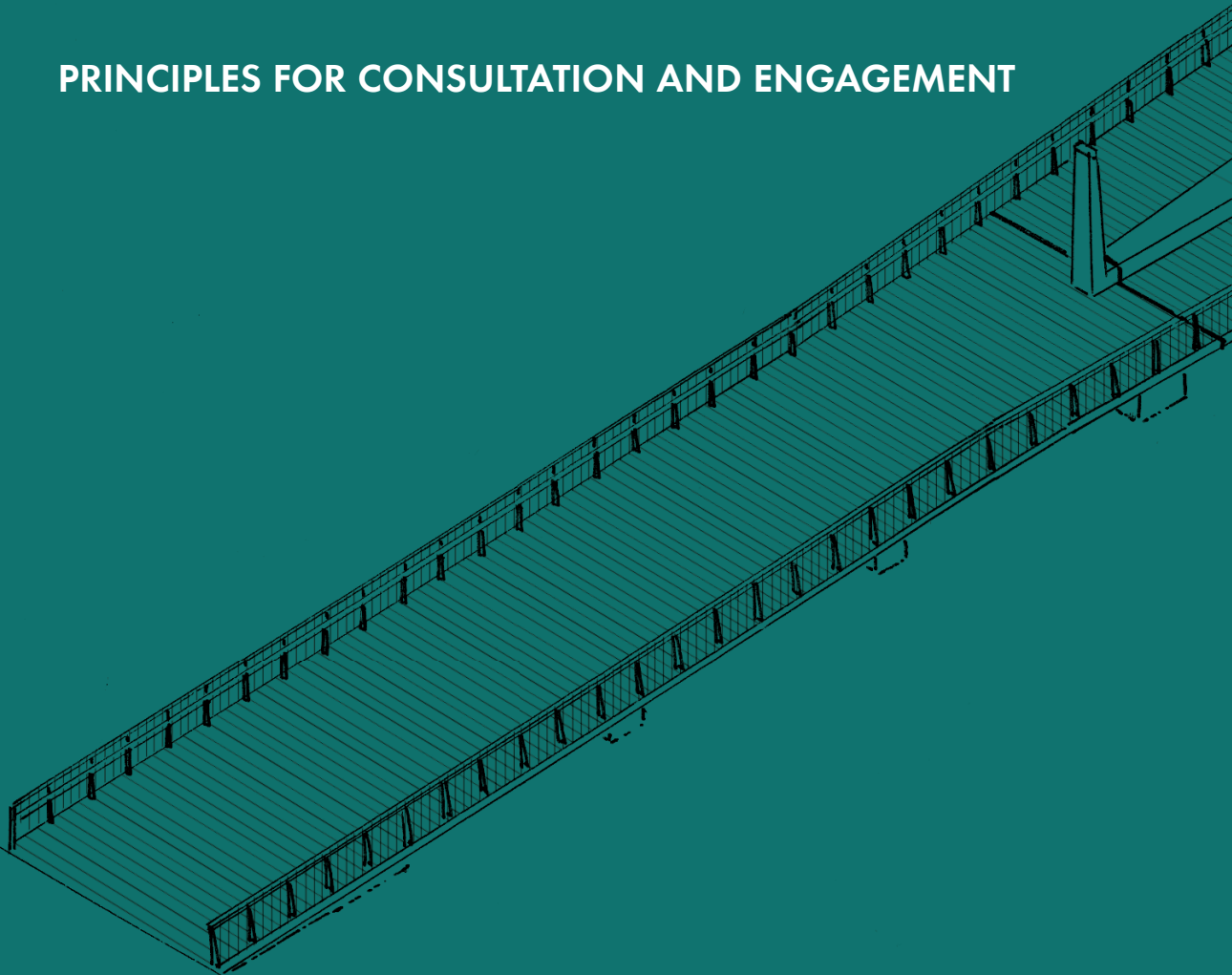
View of tracked rolling mechanism

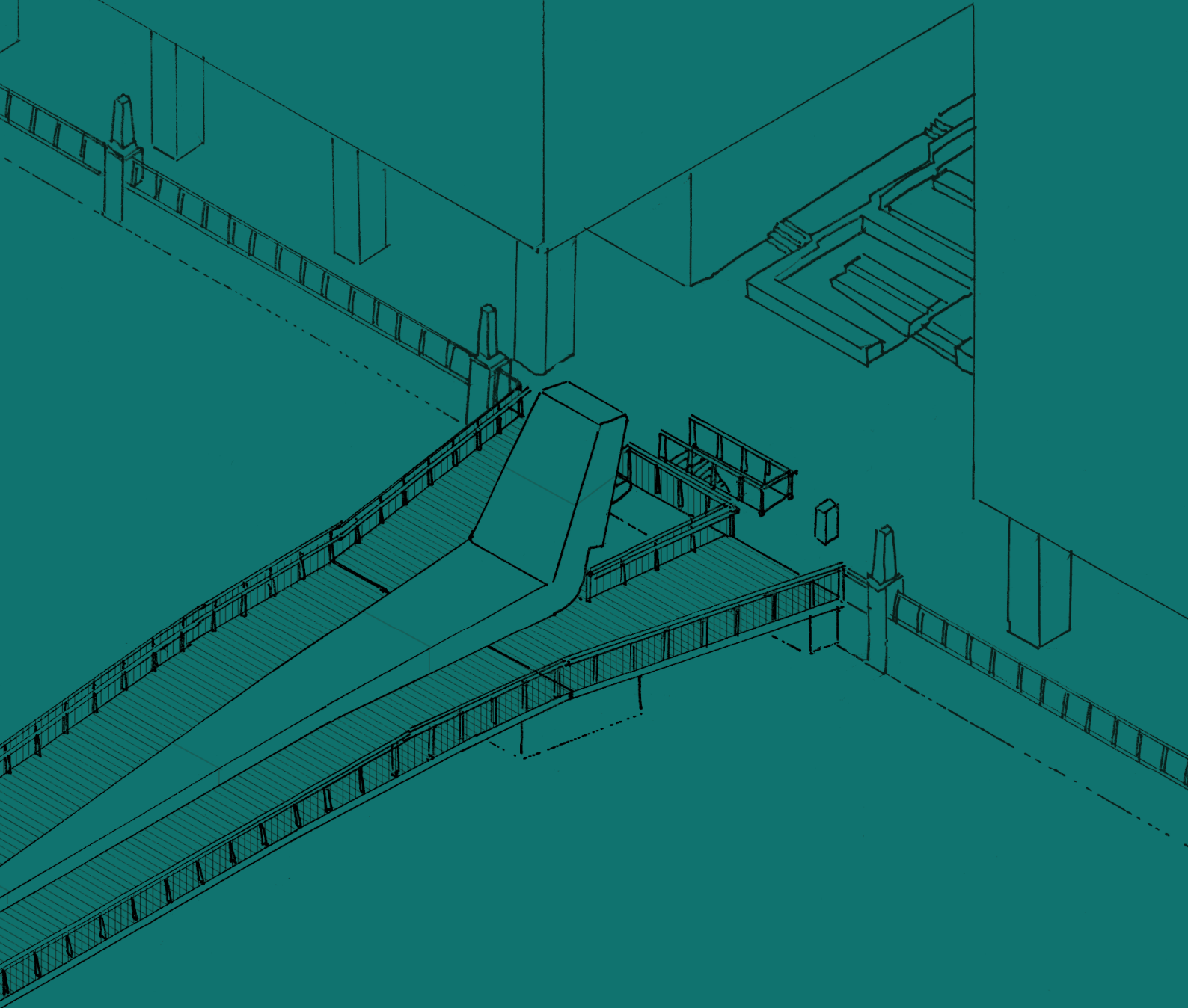


Modern rolling bascule bridge

2 PRINCIPLES OF ENGAGEMENT

2.1 PRINCIPLES FOR CONSULTATION AND ENGAGEMENT





SOUTH DOCK BRIDGE

Consultation Report
May 2018

2.1 PRINCIPLES FOR CONSULTATION AND ENGAGEMENT

The following principles have been set out for consultation and engagement as part of the South Dock Bridge project; these principles have been developed in alignment with the council's Statement of Community Involvement (September 2017) and the Tower Hamlets Community Engagement Strategy 2017-2020.

Early consultation

The Project Team will seek early public and stakeholder engagement on the South Dock Bridge project to ensure consultation is meaningful and to identify and address any issues at an early stage, as well as to put local communities at the centre of developing the project. A mapping exercise will be undertaken to identify key stakeholders and landowners. Early consultation will also be used to identify any additional stakeholders that are not identified in the mapping exercise.

Ongoing consultation

The Project Team will ensure that engagement with key stakeholders and landowners is ongoing throughout the life of the project, to build relationships, gain their input and agreement with the project proposals and ensure joined up decision making. Stakeholders and landowners will be engaged through one to one meetings (at least on a quarterly basis) and via email updates. Wider public consultation exercises will be undertaken at specific stages of the project, for example concept design. The appointed contractor will also be encouraged to provide updates during the construction of the project.

Variety of methods

A variety of techniques will be used to publicise and undertake public consultation, such as mail outs, leaflets, posters, social media, press releases and consultation events, to reach out to as many people and groups as possible. Consultation events will be arranged for a variety of times and places, such as Idea Stores and local community centres, to provide days, times and locations that are convenient for as many people as possible to attend. The consultation events will consist of exhibits of the latest project proposals and will provide an opportunity for people to ask questions about the project face to face with the Project Team.

Clear and accessible information

Information about the consultation dates and processes and consultation material will be provided in different formats, making full use of digital technology. Information about the consultation will be made available on the council's website (through a dedicated webpage) shared by the council's social media channels and communicated via posters and leaflets which will be displayed at local libraries and council buildings (including use of digital posters on screens). Consultation material will be provided in a clear and plain English manner and the purpose of the consultation will be clearly outlined. Consultation material will be exhibited at the consultation events and published on the website. Respondents will be able to respond to consultation online, via email and by post.



Inclusive consultation

The Project Team will target 'seldom heard' and marginalised groups such as young people, Black, Asian and Minority Ethnic and disabled communities, as well as other communities with protected characteristics under the Equality Act 2010, to ensure that consultation is inclusive, purposeful and reflects the makeup, needs and interests of all the different groups in the area. These communities will be reached by engaging with the local voluntary and community sector.

Internal engagement

The Project Team will publicise consultation to internal officers through appropriate channels such as th now and Yammer when it's introduced. Technical input will be sought from officers as required and the progress of the project will be reported to the South Dock Bridge Project Board on a monthly basis (the Project Board is made up of representatives from across service areas).

Members engagement

The Project Team will keep the Mayor and Councillors updated on the progress of the project through Members' bulletins and briefings. Presentations will be organised for Members at key stages of the project. Members will be briefed prior to the launch of public consultation. Members will also play a part in communicating about consultation to local communities such as at events, meetings and via social media.

Community outreach

The Project Team will work in partnership with local community groups (such as local schools) to provide them with opportunities to design and deliver aspects of the project, where it is possible to do so.

Managing expectations

The Project Team will ensure consistency in the approach to consultation and be clear about the limitations of consultation, including managing people's expectations;

Providing Feedback

The Project Team will share the outcomes of consultation and engagement, and demonstrate how the results of consultation have helped to shape the development proposals. A consultation report will be prepared after consultation exercises to summarise the key consultation responses and set out how the feedback will be taken into account.

Database

A consultation database of landowners, stakeholder and other interested parties will be maintained. Any member of the public that wishes to be kept informed of the progress of South Dock Bridge can register forwarding your contact details by email to:

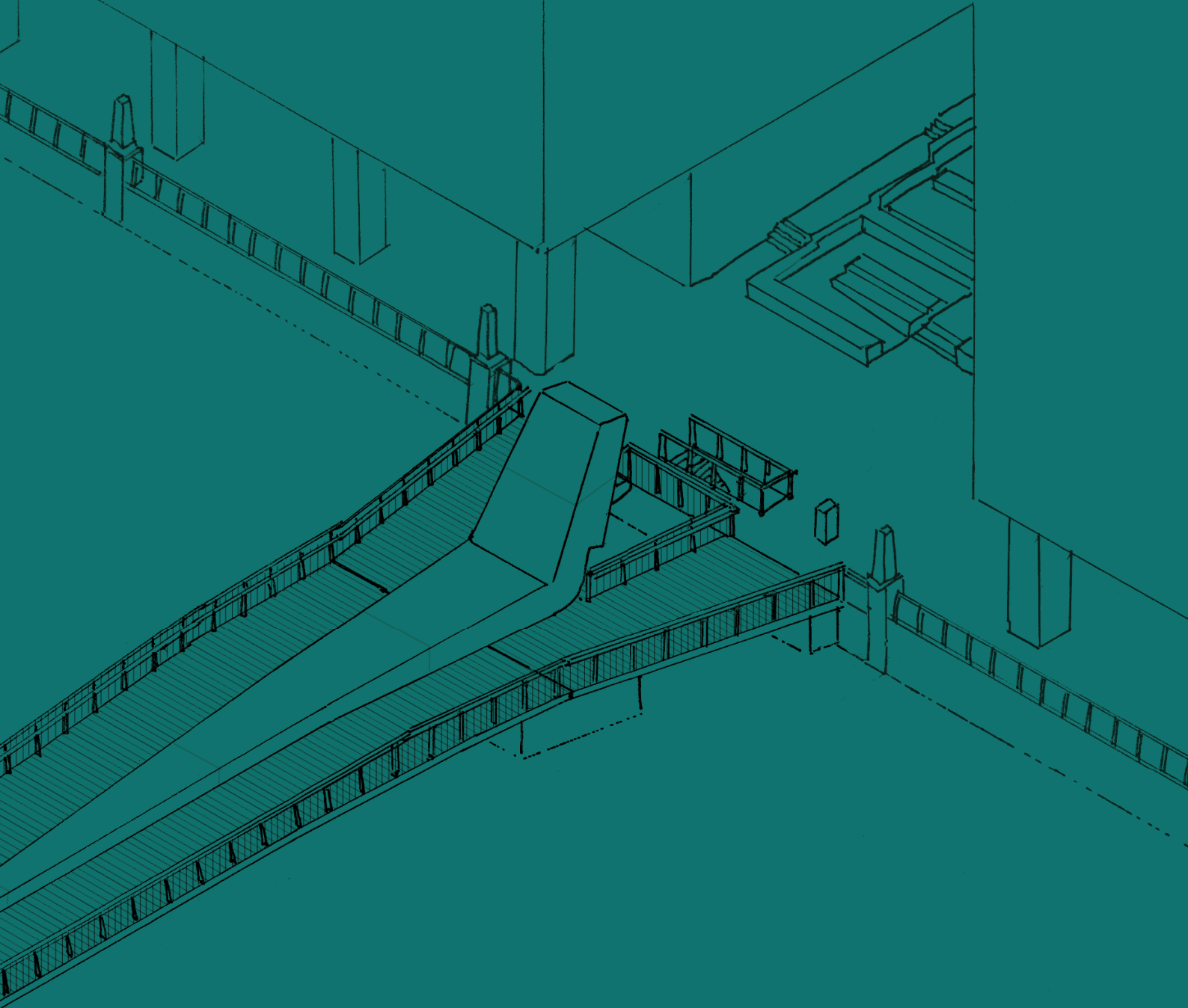
Infrastructure.Planning@towerhamlets.gov.uk, by post:

**London Borough of Tower Hamlets,
Infrastructure Planning Team
Mulberry Place
5 Clove Crescent
London E14 2BG**

or by calling us on **020 7364 2547**

3 ABOUT THE CONSULTATION

- 3.1 PURPOSE OF CONSULTATION
- 3.2 POTENTIAL OUTCOMES
- 3.3 CONSULTATION HISTORY
- 3.4 WHO WE CONSULTED
- 3.5 CONSULTATION PERIOD
- 3.6 HOW IT WAS ADVERTISED
- 3.7 METHODS OF CONSULTING
- 3.8 WHAT WE ASKED
- 3.9 METHODS OF RESPONDING
- 3.10 ANALYSIS OF CONSULTATION RESPONSES
- 3.11 STAFFED EXHIBITION DATES
- 3.12 DATES OF STAKEHOLDER MEETINGS



SOUTH DOCK BRIDGE

Consultation Report
May 2018

3.1 PURPOSE OF CONSULTATION

In order to create a bridge which delivers the best possible outcome for everyone, the council first needs to understand the aspirations of the people who will use it.

The purpose of the consultation has therefore been to:

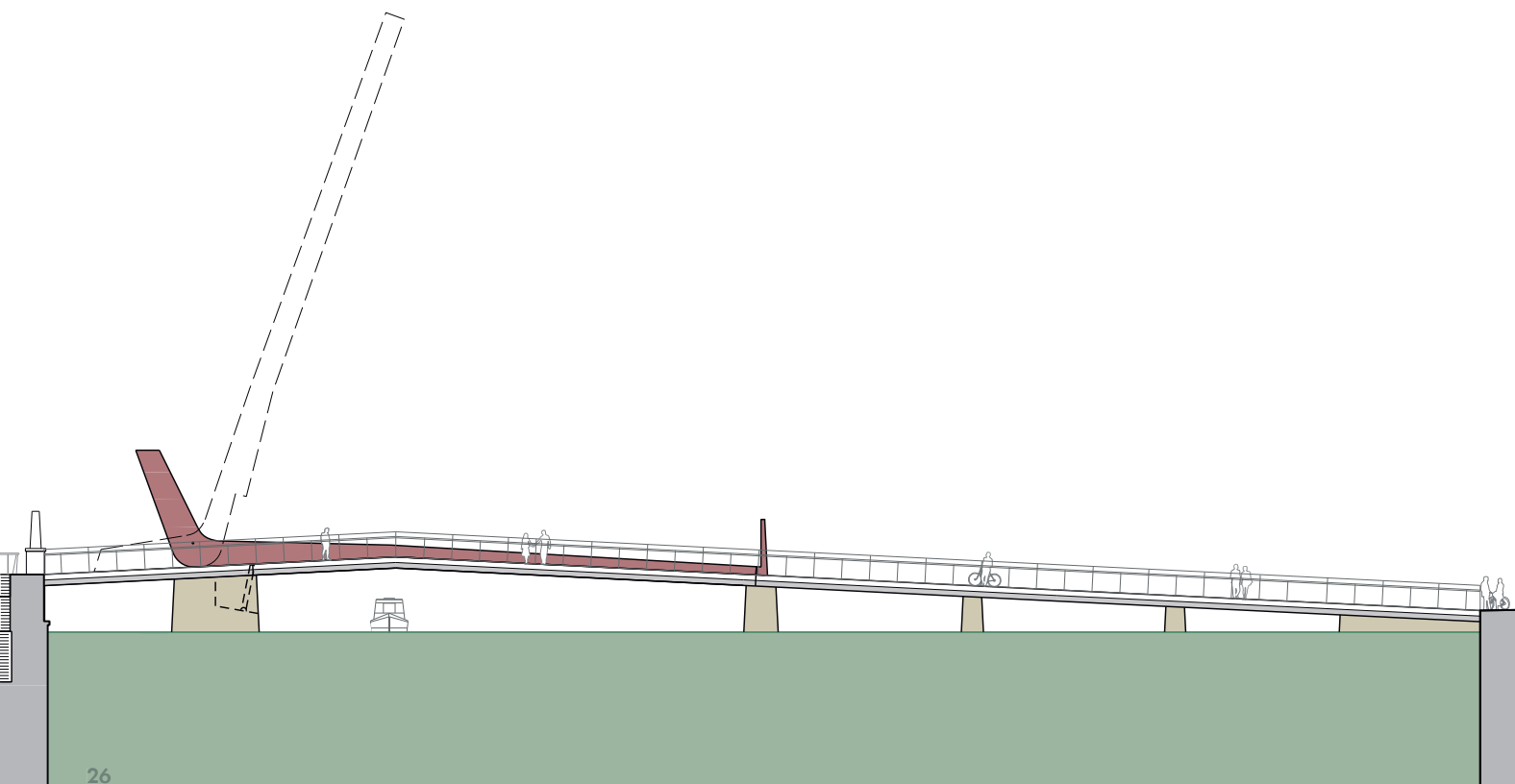
1. Raise awareness of the project and bridge design
2. Gather aspirations and understand key issues relating to the bridge
3. Be inclusive, accessible, transparent and engaging
4. Facilitate a range of ways for people to get involved
5. Clearly communicate the feedback from the community and stakeholders to the LBTH and the design team.

3.2 POTENTIAL OUTCOMES

The potential outcomes of the consultation are:

1. Following careful consideration of the consultation responses, we decide to proceed with the scheme as set out in the consultation and prepare this for a planning application
2. Following careful consideration of the consultation responses, we decide to modify the proposals to respond to issues raised and proceed with a revised scheme to planning application.
3. Following careful consideration of the consultation responses, we decide not to proceed with the scheme.

Securing full funding for the bridge will be central to its delivery. Should outcomes 1 or 2 be followed, funding commitments will impact on the overall timeframe for delivery.



3.3 CONSULTATION HISTORY

As part of the Stage 1 process, stakeholder consultation was undertaken with key stakeholders of the South Dock Bridge project, including the CRT and adjacent land owners. Stage 1 was a technical feasibility study on the location options for the bridge and did not, therefore, include public consultation.

3.4 WHO WE CONSULTED

As part of the Stage 2 process, the project team has engaged with key stakeholders and the local community to inform the design development.

The key stakeholders engaged during this process have been:

London Borough of Tower Hamlets

The LBTH commissioned this study.

The project team presented to the South Dock Bridge Project Board (January 2018), officers and Members (February 2018), and the Accessible Transport Forum (March 2018) to receive feedback on the emerging design.

Transport for London

TfL are a strategic partner in the South Dock Bridge project.

TfL supports the development of the South Dock Bridge on the Upper Bank Street alignment.

Canal & River Trust

The CRT own and manage South Dock itself, as well as the immediate dock walls where the bridge would land.

Canary Wharf Group

CWG own the land to the north of South Dock, including the landscaping on the northern approach to the Upper Bank Street alignment.

Berkeley Homes

Berkeley Homes own the land on the southern side of South Dock, where they are developing the South Quay Plaza scheme. During this phase of the bridge development (November 2017 - February 2018), the first two buildings of South Quay Plaza had begun construction – foundation work and initial storeys.

Dockland Scout Project

The Dockland Scout Project carry out water activities during the weekend, along the waterway where the bridge is proposed.

A summary of the results of these consultation events can be found in Sections 5.12 and 5.13.

3.5 CONSULTATION PERIOD

A six week public consultation was held on the draft concept designs for the South Dock Bridge, between Monday 12th February and Friday 23rd March 2018.

3.6 HOW IT WAS ADVERTISED

The consultation web pages and exhibition were advertised to the local community through a number of means, including:

1. 3,000 flyers printed and distributed to venues in the local area
2. Posters in community venues and public places in the local area
3. E-posters on the information screens at council premises and the Idea Store
4. Emails to registered interest groups
5. Leaflets distributed at the existing bridge and the surrounding area
6. Press notices and social media notifications
7. Articles published in the Tower Hamlets staff e-newsletter 'th now'

Local Councillors also tweeted news of the exhibition to followers.

Examples of these materials and advertisements can be found in Appendix B.



Consultation flyer



E-poster

3.7 METHODS OF CONSULTING

Online

Dedicated pages on the LBTH website, which set out the context for the new bridge and the draft concept designs. These pages were available for the full six week period of consultation, from 12th February till 23rd March.

Public exhibition

An exhibition was held in publicly accessible venues in the local area at key points during the consultation period. These were:

1. West Wintergarden: 11am till 3pm on Thursday 27th February
2. Alpha Grove Community Centre: 10am till 1pm on Saturday 3rd March
3. Canary Wharf Idea Store: 5pm till 8pm on Thursday 9th March

The events were staffed by members of the design team and the LBTH project team so that visitors were able to ask questions about the plans and design, and seek clarification on any points of interest.

The exhibition was visited by approximately 100-130 people across the three events, with the West Wintergarden being the most highly visited due to its location on a walking desire line, with strong pedestrian flows.



Location of Public Consultation events





3.8 WHAT WE ASKED

Our consultation sought views on the following:

1. Support or opposition for the introduction of a new pedestrian / cycle bridge over South Dock
2. Likelihood of using the new bridge
3. The treatment/surface finish of the steel the bridge is to be constructed from
4. The preferred example of bridge precedent options provided, and why
5. If places to sit, or a central viewing platform on the bridge would be a valuable addition
6. What the most important aspect of the bridge is, selected from a range of options

Questions were also asked about the consultees, including:

7. Relationship to area (live/work/socialise)
8. Gender
9. Age bracket
10. Home postcode
11. Ethnicity

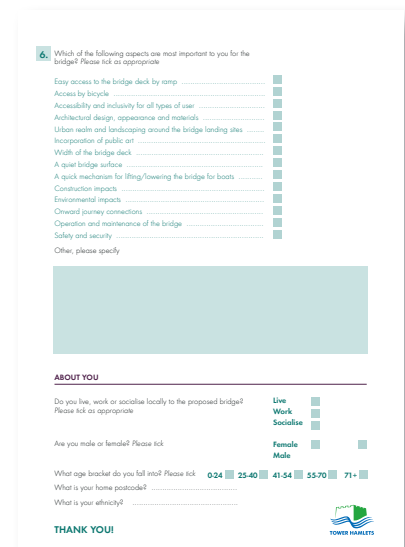
A full list of questions can be found in Appendix A

3.9 METHODS OF RESPONDING

Residents and community members were given several ways to comment on the draft proposals and designs, including:

1. Feedback forms provided at the exhibitions to be dropped in a dedicated drop box
2. Online feedback forms provided alongside the web content
3. By email to a dedicated project email address
4. By letters and/or feedback forms posted to the Infrastructure Planning Team at LBTH

In total, **161 feedback forms** were completed, and **11 supplementary email** submissions were received.



3.10 ANALYSIS OF CONSULTATION RESPONSES

Answers to all closed questions were reviewed and the results tabulated and reported. These responses have been presented in graphic form in the following section.

Where respondents provided additional comments, these were read and analysed in detail and have been summarised and presented in the following section.

3.11 STAFFED EXHIBITION DATES

There were three staffed exhibition days altogether, the details of which are below:

West Wintergarden

1am till 3pm on Thursday 27th February

Alpha Grove Community Centre

10am till 1pm on Saturday 3rd March

Canary Wharf Idea Store

5pm till 8pm on Thursday 9th March

3.12 DATES OF STAKEHOLDER MEETINGS

Canary Wharf Group

Friday 1st December 2017

Thursday 18th January 2018

Canal & River Trust

Monday 15th January 2018

Berkeley Homes

Friday 24th November 2017

Thursday 18th January 2018

Tuesday 13th February 2018

Dockland Scout Project

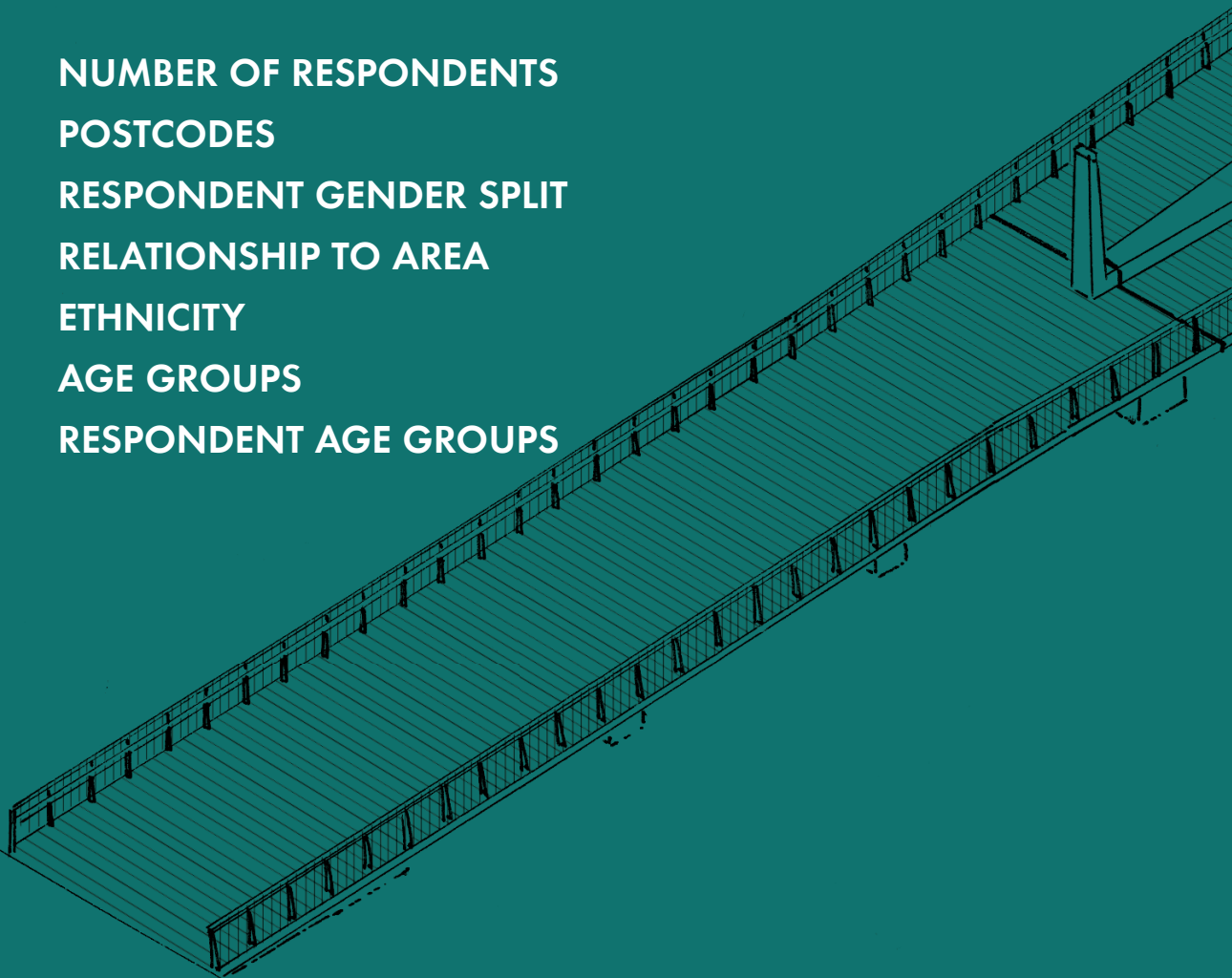
Wednesday 18th April 2018

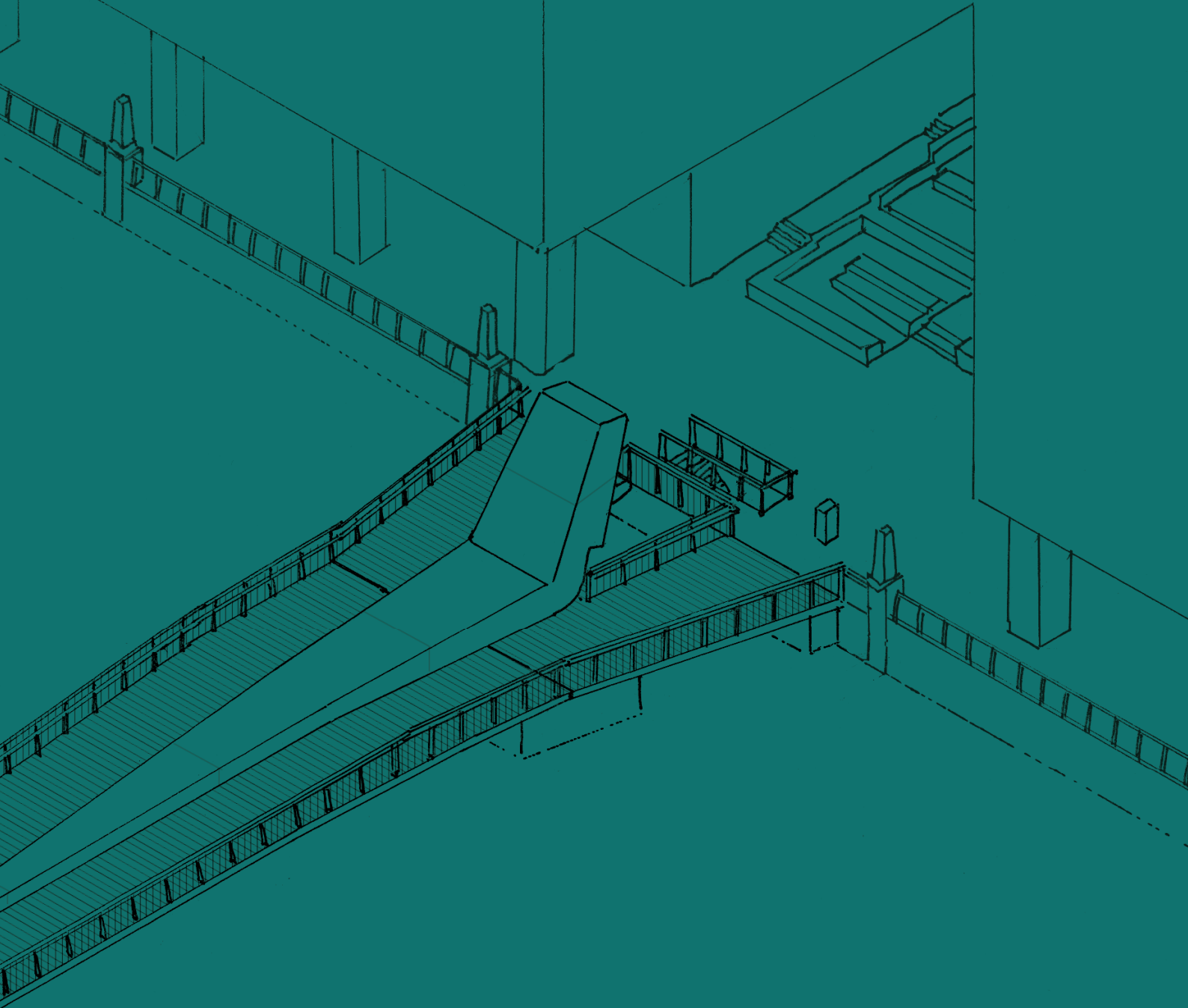
Accessible Transport Forum

Thursday 15th March 2018

4 ABOUT THE RESPONDENTS

- 4.1 NUMBER OF RESPONDENTS
- 4.2 POSTCODES
- 4.3 RESPONDENT GENDER SPLIT
- 4.4 RELATIONSHIP TO AREA
- 4.4 ETHNICITY
- 4.5 AGE GROUPS
- 4.6 RESPONDENT AGE GROUPS





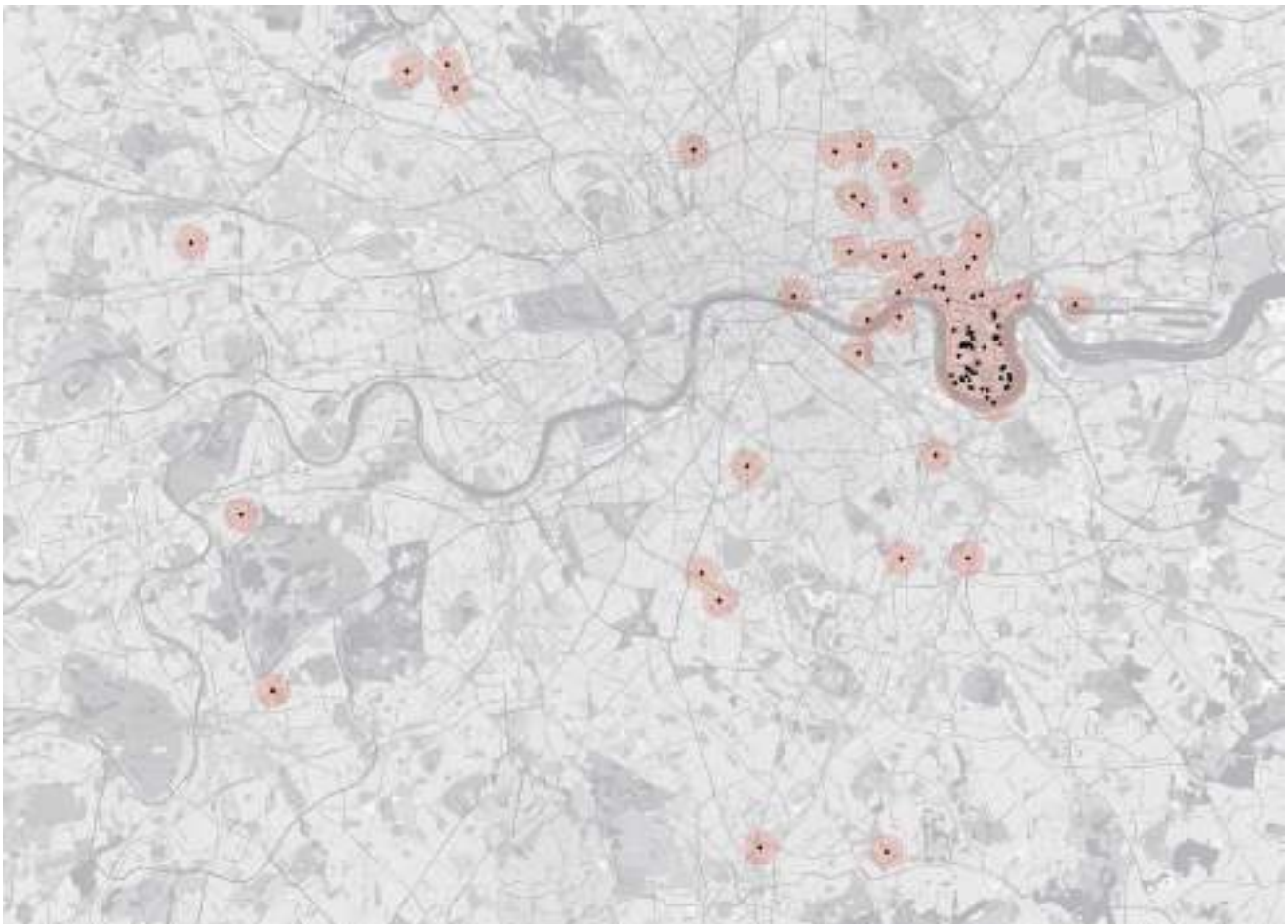
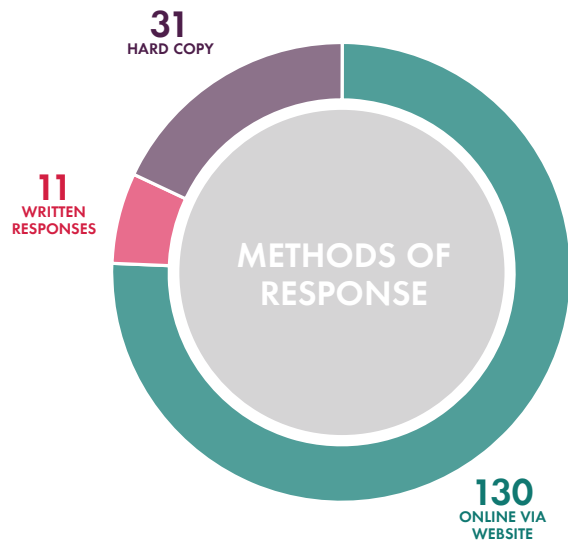
SOUTH DOCK BRIDGE

Consultation Report
May 2018

4.1 NUMBER OF RESPONDENTS

Over the six week period of consultation, we collected 161 completed feedback forms from the public consultation events, and online through dedicated pages on the LBTH website. We also received 11 email responses from individuals and groups. This gives a total response rate of 172.

The most popular method of responding proved to be via the council's dedicated website page, which was the source of **76%** of feedback forms.



The majority of registered respondents' postcodes were from the local Isle of Dogs area

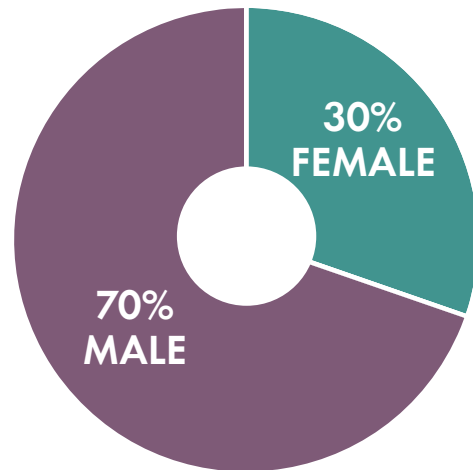
A map of the United Kingdom showing the distribution of respondents. Red circles of varying sizes indicate the locations of respondents. The largest circle is centered on London, with several smaller circles in the surrounding region. A single circle is located in the northwest of England, and another is in the west of Scotland. The map is overlaid with a grid of latitude and longitude lines.

4.2 POSTCODES

Of those respondents who recorded their home postcodes, the majority of feedback was received from those who live locally, with a number of respondents coming from the wider London area too. However, one respondent had come as far as Glasgow in Scotland.

4.3 RESPONDENT GENDER SPLIT

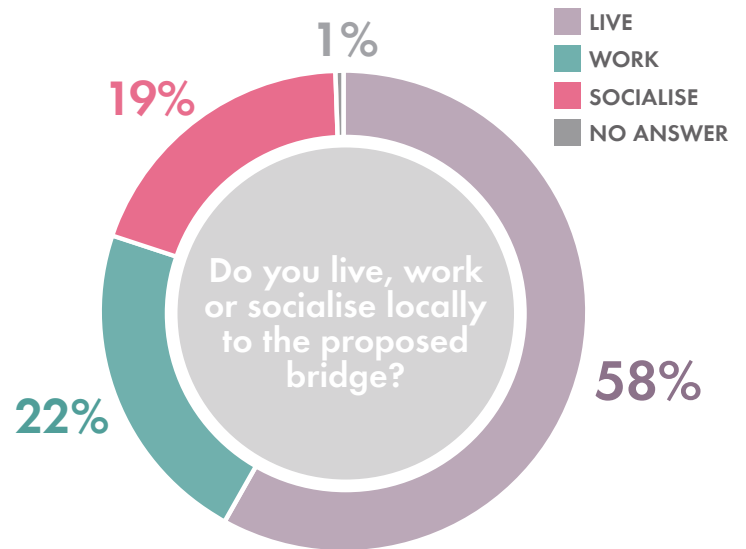
Of those who responded, 70% of consultees identified themselves as male. A small number of responses for this question were void as for a short time, the online form gave yes/no response options rather than male/female.



4.4 RELATIONSHIP TO AREA

Consultees were asked what their relationship was to the area around the proposed bridge, and were asked to select whether they live, work or socialise locally.

58% of respondents said they lived locally to the proposed bridge. Of these people, 13% said that they also worked and/or socialised in the area too. The geographical spread of responses is shown below.



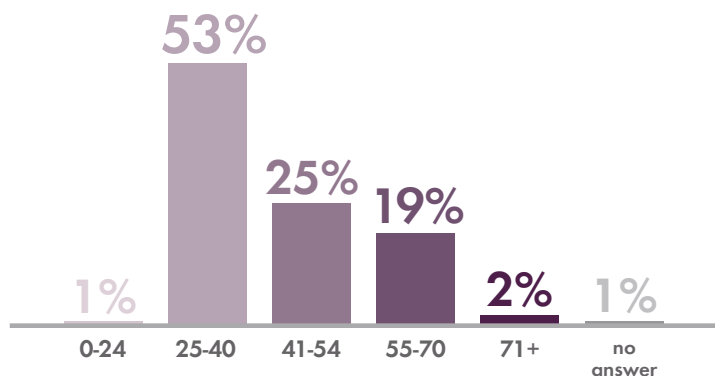
4.5 ETHNICITY

Respondents were asked to provide their ethnicity, with this being an open question rather than a selection from a checklist. Of respondents who answered this question, the majority were White British/Irish, Asian British or mixed.

A number of alternative responses were given including Cypriot, Iranian and world citizen. A graphic representation of the results is shown below.

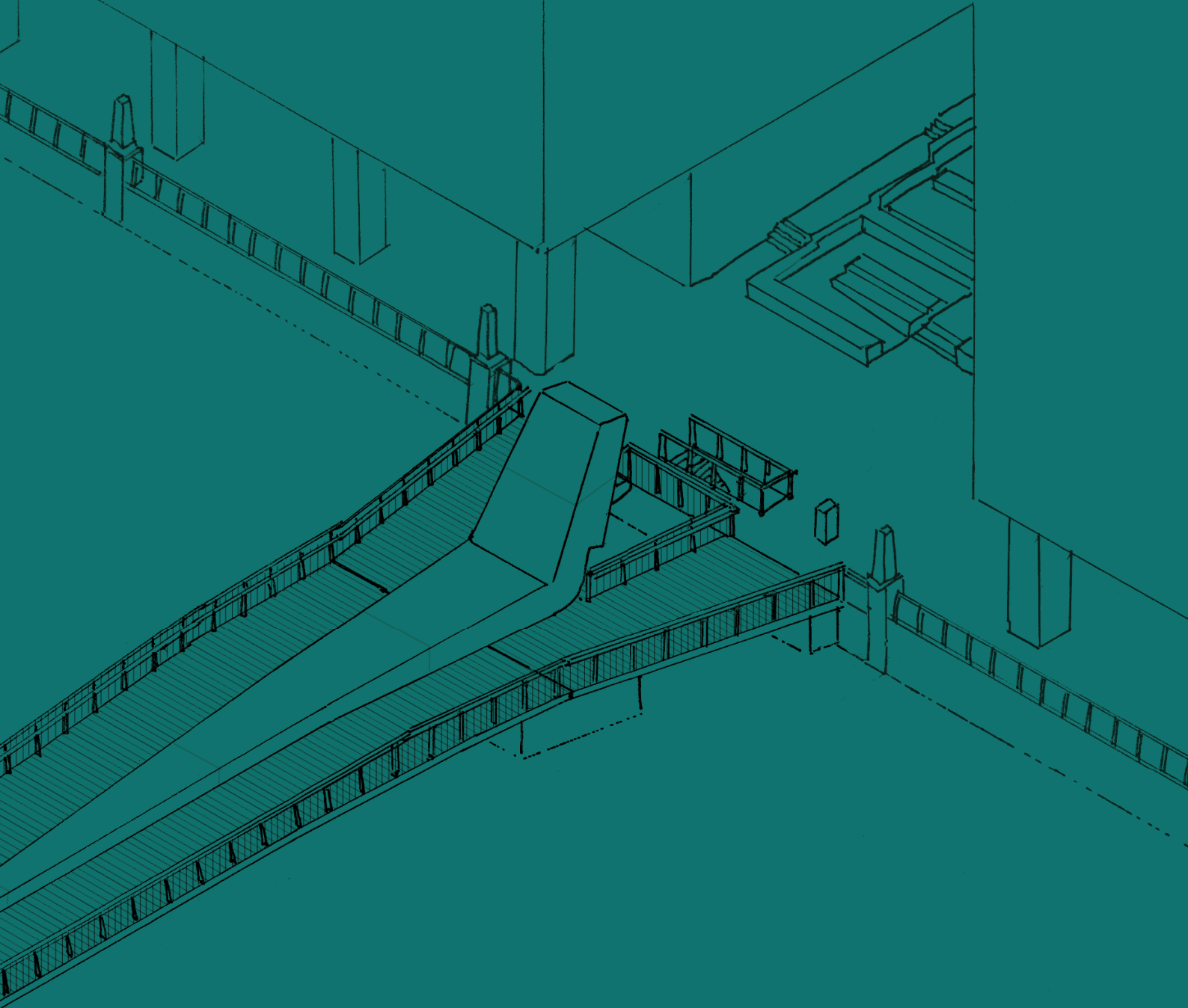
4.6 RESPONDENT AGE GROUPS

Respondents' ages ranged from under 24 to over 71, with the vast majority of those who responded falling within the 25-40 age bracket.



5 SUMMARY OF ALL RESPONSES

- 5.1 SUPPORT FOR THE BRIDGE
- 5.2 USE OF THE BRIDGE
- 5.3 FREQUENCY OF USE
- 5.4 MODE OF USE
- 5.5 MATERIAL PREFERENCE
- 5.6 EXAMPLE BRIDGE PREFERENCE
- 5.7 SUPPORT FOR SEATING ON BRIDGE
- 5.8 PRIORITIES FOR BRIDGE DESIGN
- 5.9 SUMMARY OF DISCUSSIONS AT EVENTS
- 5.10 SUMMARY OF ADDITIONAL FORM COMMENTS
- 5.11 ISSUES RAISED IN WRITTEN CORRESPONDENCE
- 5.12 SUMMARY OF MESSAGES FROM LBTH MEETINGS
- 5.13 SUMMARY OF MESSAGES FROM STAKEHOLDER LIAISON



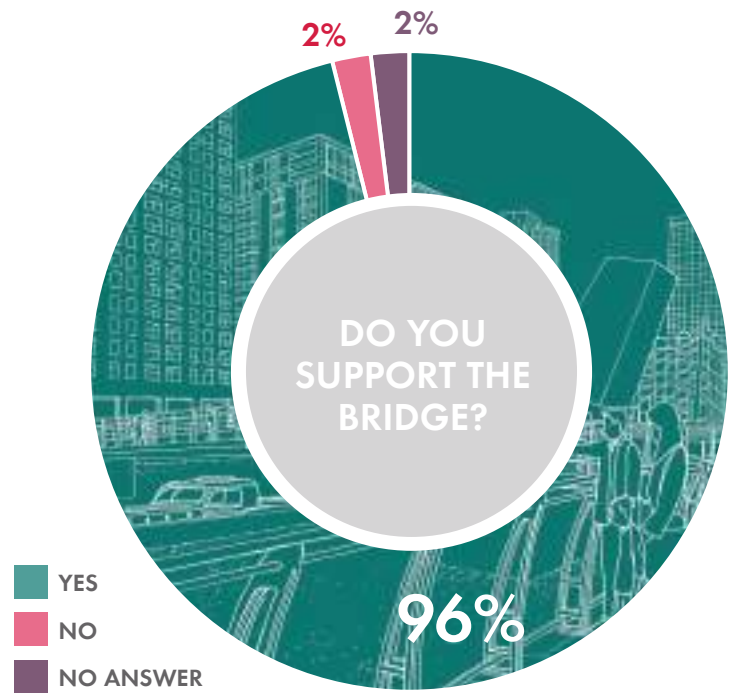
SOUTH DOCK BRIDGE

Consultation Report
May 2018

5.1 SUPPORT FOR THE BRIDGE

Of those who responded, the vast majority of people (96%) said that they **support the introduction of a new pedestrian/cycle bridge** over South Dock in principle.

Two people did not support the bridge in principle. One of these people lives locally in South Dock and one lives outside of London but works locally.

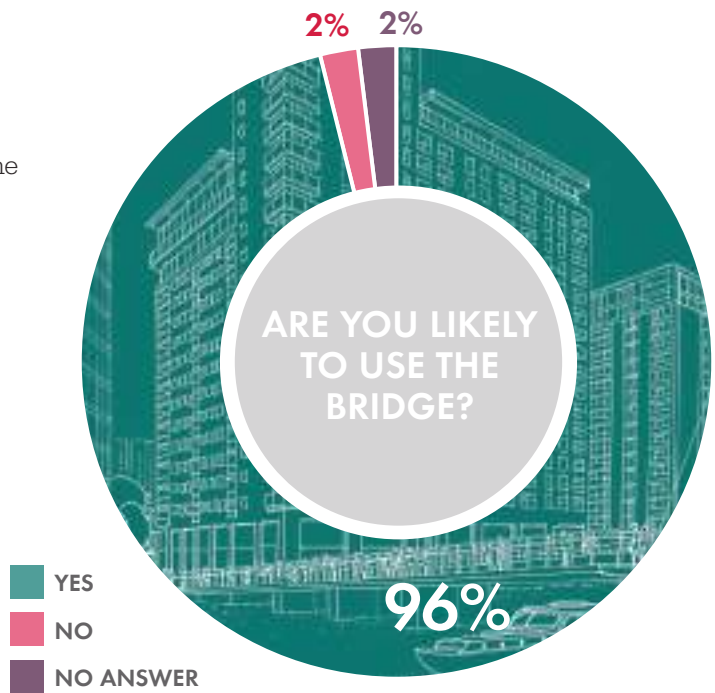


5.2 USE OF THE BRIDGE

The responses of those who are likely to use the new bridge are virtually identical to those who support the introduction of the new bridge.

Two respondents said that they would not use the bridge, and these were residents in Essex and Glasgow.

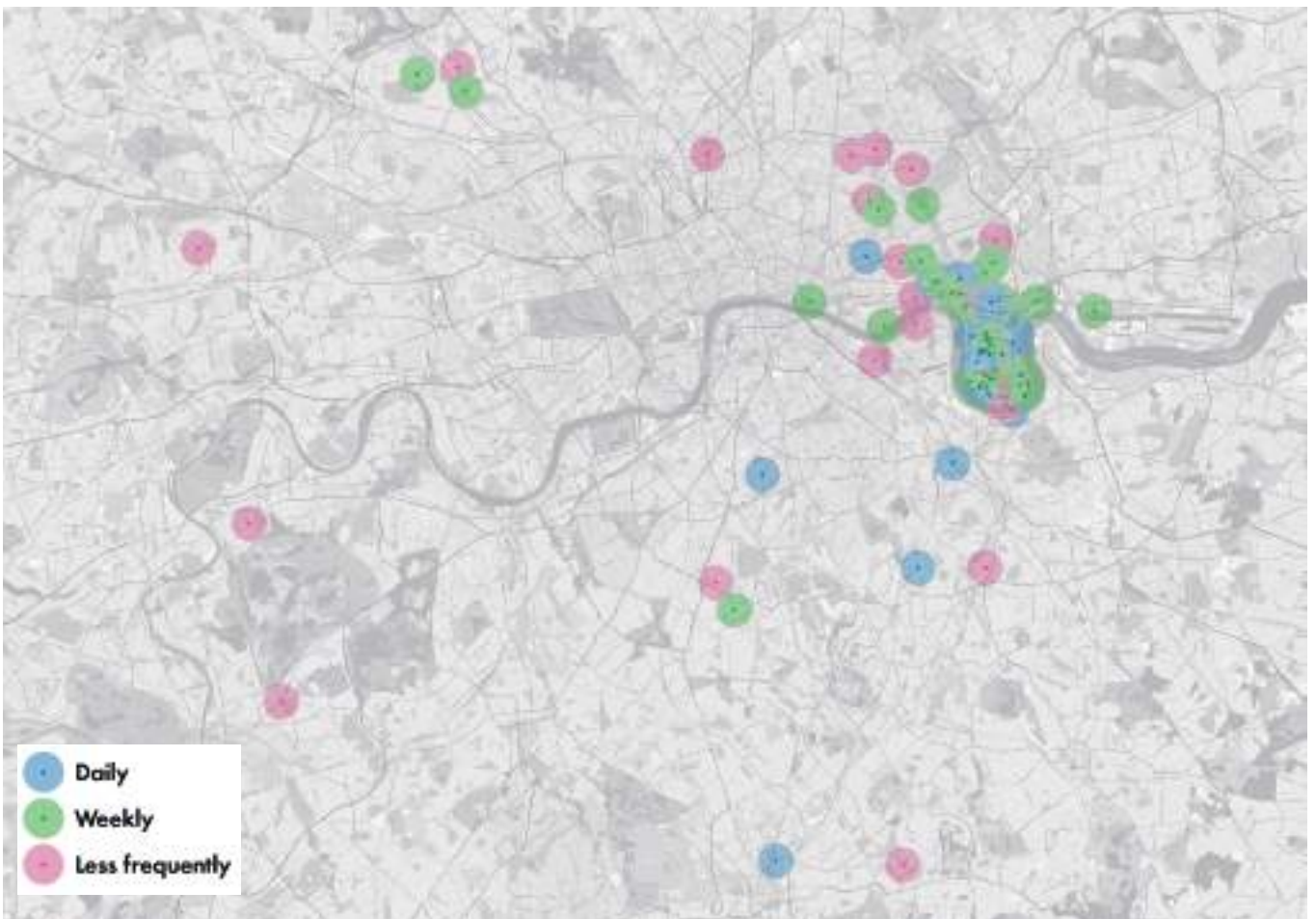
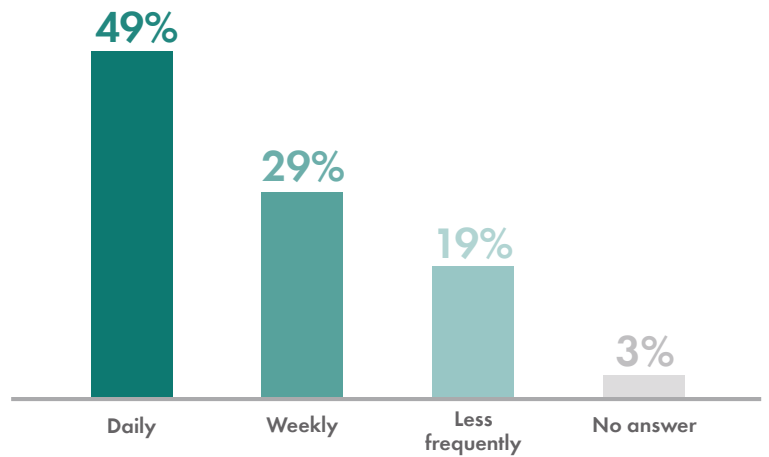
Only one respondent mentioned that they neither support, nor would use the bridge.



5.3 FREQUENCY OF USE

Consultees were asked how often they think they would use the bridge, once it is in place and, of those who responded, 49% believed they would use the bridge **on a daily basis**. 29% believed they would use the bridge on a weekly basis and 19% thought they would use it less frequently.

Overall, this suggests that demand for the bridge will be reasonably high.

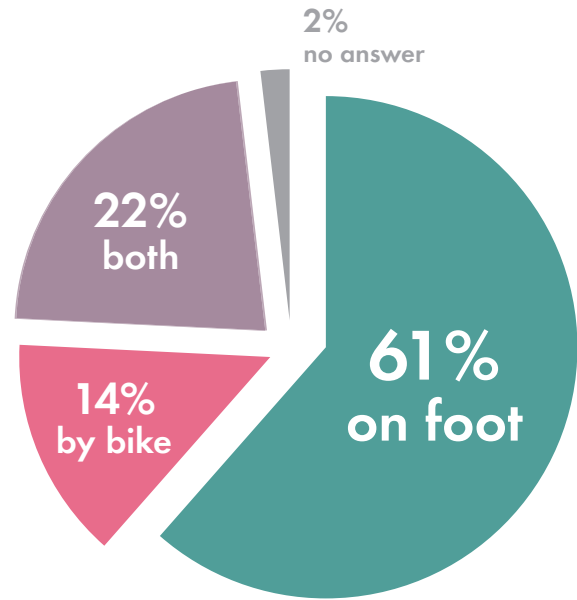


5.4 MODE OF USE

Consultees were asked if they were likely to use the new bridge, and if so, would they be likely to cross the bridge by bike or on foot.

The majority of those who responded (61%) said they would be likely to use the new bridge **on foot**. However, respondents were also likely to use the bridge by bike and on foot.

14% of those asked said they would use the bridge by bike alone.



5.5 MATERIAL PREFERENCE

The feedback form informed consultees that the proposed bridge will be constructed from steel but the surface finish has yet to be determined.

The feedback form presented a range of images from which to choose a preferred treatment of bridge material, including raw metal, a bright tone, a dark tone or a neutral tone. The results are shown in the infographic below.

The **neutral tone** was the preferred choice with 40% of those who responded selecting this treatment over the other three options. The remaining options of raw, bright or dark all received similar levels of support in the high-teens.



raw steel



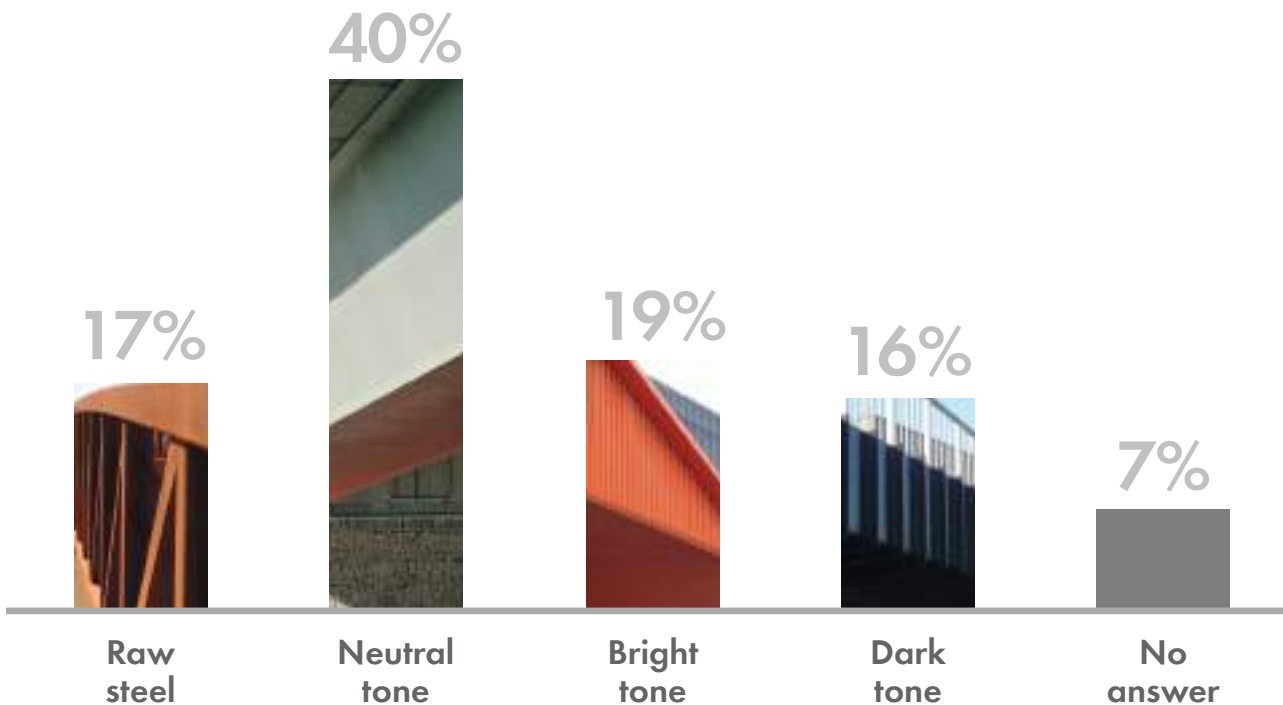
a bright tone



a dark tone



a neutral tone



5.6 EXAMPLE BRIDGE PREFERENCE

Three examples of bridges from around the world were shown on the consultation website and at the exhibition.

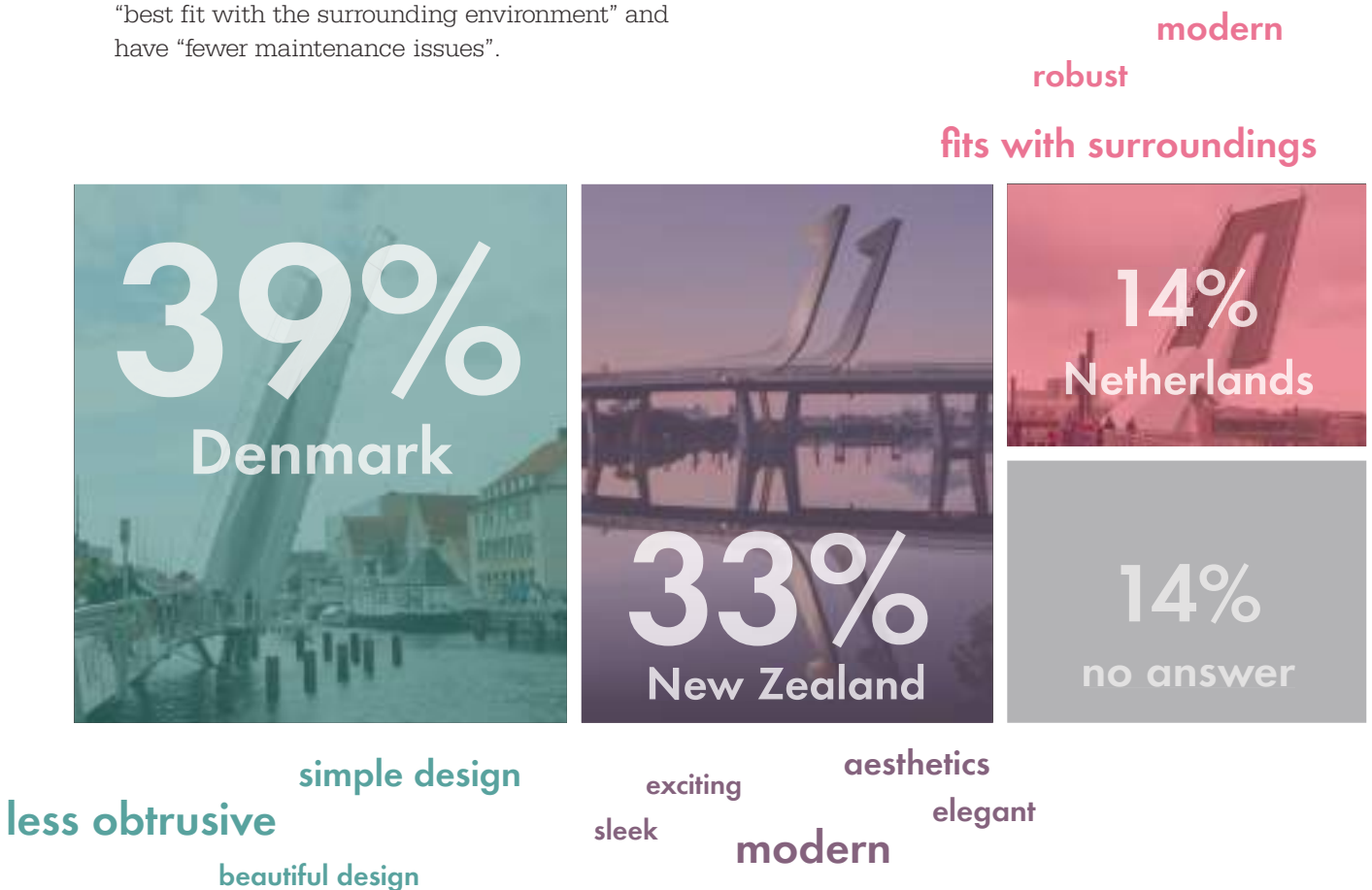
Consultees were then asked to select which example they preferred and explain why. The results are shown in the infographic below.

Consultees preferred the example from **Denmark** in general, with those who chose it explaining that they preferred it because of its “simple” and “beautiful” design, which appears “less obtrusive” than the other options, and “complements the existing bridge” and is most likely to “aesthetically fit into the surrounding area”.

The example from New Zealand was also popular because of its “aesthetics” and “modern and sleek” appearance. It was also considered to “best fit with the surrounding environment” and have “fewer maintenance issues”.

The example from the Netherlands was less popular, with people who chose one of the alternatives indicating it was “ugly” and “too blocky and bulky”. Those who selected the Netherlands bridge as their preferred option found the bridge looked more “robust”, “strong” and “solid” and appreciated the fact it was “wide” with “benches for those who want to stop to take a call or for tourists who stop to take pictures”.

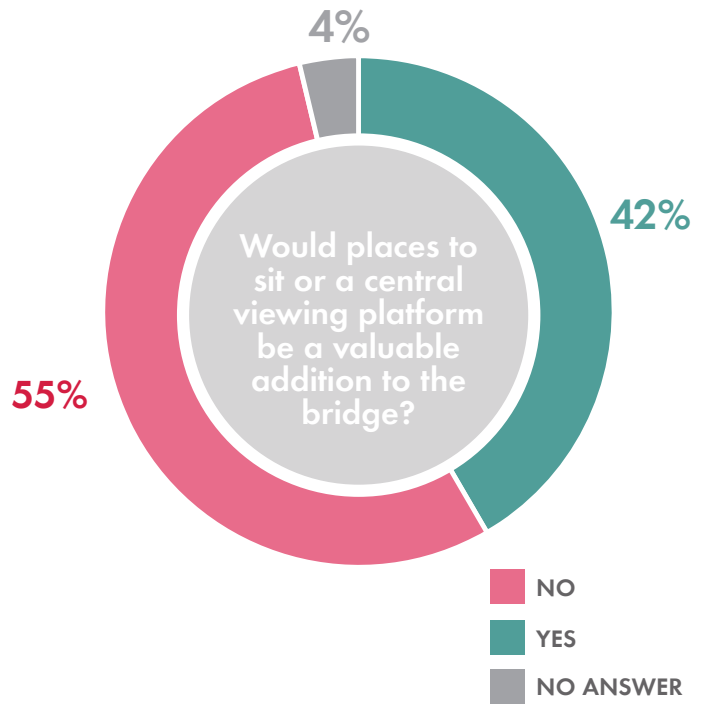
Three of the respondees commented that they found it hard to tell which design they preferred because the consultation pictures and descriptions were insufficient in detail.



5.7 SUPPORT FOR SEATING ON BRIDGE

Respondents were asked whether they believed there could be scope for creating places to sit on the bridge, or a central viewing platform, and whether they considered this to be a valuable addition to the bridge. Of those who responded, a small majority suggested that seating or a viewing platform **would not be a valuable addition** to the bridge.

The results are shown in the infographic to the right, with the geographic spread of responses below. This indicates a general pattern of support for seating from those who live further from the proposed bridge location, perhaps because they view the bridge more as a 'leisure' connection rather than commuter route.

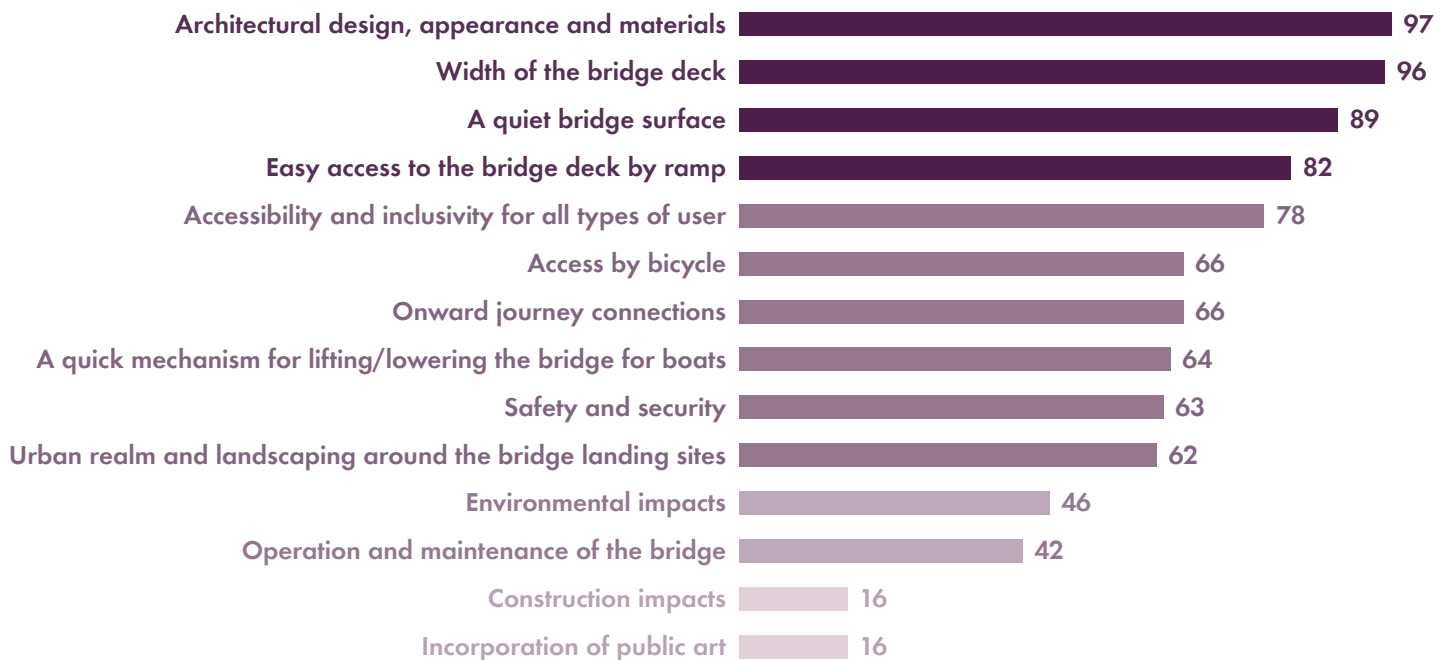


5.8 PRIORITIES FOR BRIDGE DESIGN

Consultees were asked which aspects of the bridge are most important to them, and were given a range of options to choose from which they could select multiple responses.

The results are shown in the infographic below.

Respondees felt that the **architectural design, appearance and materials** of the bridge was the most important aspect to them, as well as the width of the bridge, a quiet bridge surface and easy access to the bridge deck by ramp.



5.9 SUMMARY OF DISCUSSIONS FROM EXHIBITION EVENTS

Westwinter Garden

Allies and Morrison In attendance: Greg Holmes, Laura Dodds-Hebron & Conor White Gibson
Tower Hamlets In attendance: Jaskaren Mahil-Sandhu, Jonathan Morris, Nazim Rahman

Key issues raised

1. Separation of cycle and pedestrian routes over the bridge were desired by a number of people
2. Deck noise of proposed bridge is a key concern to residents of Discovery Dock East (Anecdotal evidence from a resident of Discovery Dock East on the excessive noise pollution from the Wilkinson Eyre bridge desk)
3. CWG support the bridge in principle but did press that CWG is opposed to the bridge operating as a primary cycle route and is, as such opposed to a dedicated cycle lane over the bridge into Canary Wharf.
4. East London Sea Scouts attended and highlighted their opposition to the proposed bridge on the grounds of it reducing their access to the western end of the South Dock. As a result of discussions, they are to setup a channel of dialogue with Tower Hamlets and Canal & River Trust to highlight their concerns.

Alpha Grove Community Centre

Allies and Morrison In attendance: Laura Dodds-Hebron & Conor White Gibson, Greg Holme
Tower Hamlets In attendance: Jaskaren Mahil-Sandhu, Sajid Amirinia

Key issues raised

5. Again the main issue was the desired separation of cycle and pedestrian routes over the bridge, with anecdotal evidence that “most cyclists do not dismount when cycling across the existing bridge”
6. President of the Isle of Dogs Neighbourhood Planning Forum committee and of the Residents Association of Pan Peninsula Towers attended and highlighted a number of issues. He called for the segregation of cycling and walking on the bridge; highlighted the issue of high winds at times blowing across the South Dock; and issues with the density of consented schemes to the south of the dock.
7. There were comments about the consultation not being well published and people only having heard about it through social media.

Ideas Store, Canary Wharf

Allies and Morrison In attendance: Louise Mansfield & Conor White Gibson

Tower Hamlets In attendance: Jaskaren Mahil-Sandhu

Key issues raised

8. Once again the main issue was the desired separation of cycle and pedestrian routes over the bridge, with anecdotal evidence from a local cyclist that Manchester Road does not receive a lot of cycle use due to a pinch point over the existing Blue Bridge. It was felt that “there will be a lot of cyclists using this new bridge”.
9. Contrasting anecdotal evidence from visitors did suggest that the existing bridge works reasonably well with non-segregated cycling and walking, and that it may not, therefore, be an issue for the new wider bridge.
10. The Tenant representative from Discovery Dock East attended and was largely in support of the proposed bridge but highlighted issues with the southern approach route due to its current use as a turning point for vehicles into the Discovery Dock East basement. He also highlighted a number of issues the residents have with the under-construction South Quay Plaza, namely in the on-going debate over the basement link to the Berkeley Homes scheme and amount of vehicle traffic likely upon completion. He also raised concerns over the placement of the proposed children’s play space at South Quay Plaza adjacent to vehicular movement routes.
11. A point was raised about the area not being suitable as a primary cycle route and cyclists should be diverted.

5.10 SUMMARY OF ADDITIONAL COMMENTS ON FEEDBACK FORMS

The additional comments on the feedback forms highlighted three key areas of concern from respondents, which were:

1. Movement on and around the bridge
2. Safety and security
3. Continued access to the dock by dinghies

The issues raised in relation to these areas are outlined below.

Movement and access

Three respondents noted concerns that it will be dangerous for cyclists and pedestrians to share non-segregated space on the bridge, with one respondent noting that cyclists tend to assume they have preferential use of shared space. Suggestions for addressing this issue included removing cycles from the bridge, so that it is pedestrian only, and allocating one of the existing and new bridges as a pedestrian bridge and one as a cycle bridge.

One person noted that seating on the bridge would be fine, providing that it does not take space from movement channels on the bridge. It would need to be incorporated in space that couldn't otherwise be used by pedestrians or cyclists. Reference was made to people who currently pause to take photos on the existing bridge, and cause chaos due to the busy flow of people.

Wider movement concerns included:

4. that the access to the bridge must be spacious and without obstructions (as the current bridge has at the northern bank);
5. that the cycle route should be well-integrated with the surrounding cycle network; and
6. that movement to the southern approach should not affect parking for Discovery Dock residents, particularly access to the car lift

Safety and security

Safety and security on the bridge was a key concern for respondents.

Three respondents raised concerns relating to 'pan handlers' and homeless people on the bridge, which is apparently an issue for the existing bridge. Some respondents suggested this can be intimidating and one person highlighted the potential danger of cyclists colliding with people who are stationary on the bridge.

Lighting on the bridge was highlighted as an additional priority by one respondent, to ensure the bridge feels inviting, safe and secure. One person noted that seating on the bridge could lead to anti-social behaviour, if large groups of people or gangs congregate on the bridge.

Access to the dock by dinghies

Four respondents flagged the issue of current use of the dock by the Scout Sailing Group at weekends and the requirement for their access to continue. Suggestions to maintain access included making the bridge height 8m so dinghies can continue to sail beneath; and leaving the bridge mechanism open on Saturdays for a few hours, while the Scouts' activities take place.

Other bridge characteristics

In addition to those respondents who highlighted a quiet bridge surface in their priorities, one person highlighted this as a high priority in their additional comments, on the basis of the existing bridge being too noisy.

One person highlighted the consideration of wind conditions for the bridge, wishing to ensure a comfortable environment for users.

One person noted that whilst they support the principle of a new bridge, they are not convinced by the proposed specific location.

Process comments

One person noted that they felt the consultation was poorly advertised and mentioned that they only learnt about the events through a neighbour, who was made aware by social media.

The respondent suggested that, in future, the LBTH could use the leaflet stands in Canary Wharf shopping mall to advertise events in advance.

5.11 ISSUES RAISED IN WRITTEN CORRESPONDENCE

Local resident

1. The West India Docks were among the most important docks in London/UK, they were once working docks handling huge amounts of cargo. The proposed bridge needs to respect this heritage, it needs to look like it belongs in this location – a bridge design in harmony with the dock cranes for example;
2. **The views east/west along South Dock are important.** Unfortunately a number of developments have compromised these views, nevertheless what is left of these views is worth preserving. This **proposed bridge shouldn't be too imposing.** The existing footbridge achieves this rather well, it is a lovely design, but not overbearing, it is as easy to look through it as it is to look at it;
3. I should imagine some people will wish to be protected from the elements – seeking solid sides and a roof to the bridge. I do hope this doesn't happen. This is a relatively short walk and such **a solid structure would almost certainly create a visual barrier**, breaking up the South Dock into two distinct parts;
4. The deck of the proposed bridge needs to be as **non-slip as possible**, especially given it is sloping. The existing bridge was originally decked with timber, this became very slippery and dangerous and had to be replaced. The current metal deck is much better, but could probably be improved upon.
5. Movement of large vessels through the bridge is important, even if only an occasional occurrence. It appears the slope of the new bridge has been designed to **allow sufficient headroom that the movement of most dock craft can pass** and repass without the need to open the bridge. I very much support this approach.
6. I am concerned that CRT appear to be seeking a number of spreader pontoons as part of their compensation package. The docks are already extensively taken up with buildings and other

structures – **further structures in the water simply add to the 'infill' of the docks and should be resisted;**

7. **A floating pedestrian link bridge across the Millwall Cut should be considered.** This would not only increase pedestrian capacity from the South Dock Bridge to the community to the east, but would also ease the pressure on footpaths immediately to the south of the new bridge. A floating pedestrian link bridge could easily be swung out of the way to allow boats to enter/exit the Millwall Cut. It would also aid ship mooring through enabling the mooring of large craft to the east of the South Dock Bridge, enabling them to overhang the Millwall Cut;
8. I am a cyclist and I doubt very much that segregation of cycles and pedestrians will work, I therefore **support the non-segregation approach.** However I also think **cyclists should be encouraged not to use the new South Dock Bridge through the provision of better alternative routes.** For example, a cycle route from the Blue Bridge across Wood Wharf should be quicker and so reduce cyclists desire to use the South Dock Bridge. I think part of a strategy to encourage alternative routes for cyclists may be to make the route on the north side of the bridge a bit of a challenge for them through retaining the existing zig-zag ramps.

Local resident

This is a **great idea and is very much needed.** However, equally as important is a **footbridge over Millwall cutting to link South Quay walk with Thames Quay.** I understood that this was to be delivered with the new development at South Quay but I can't find any information about it. Currently, pedestrians have a 250 metre detour up to Marsh Wall and back if they wish to continue walking along the southern edge of South Dock.

Local resident

9. Locally, **cyclists and pedestrians are not good at sharing space**. In the Greenwich foot tunnel, there are numerous incidents of cyclists (normally at commuting time) cycling too fast in a constrained space and injuring pedestrians. Equally, there are incidents of pedestrians sabotaging cyclists. Implementing a new project where cyclists and pedestrians are expected to live together is very optimistic and I would urge you to **look again at having separate spaces for cyclists and pedestrians**.
10. If you're not having separate spaces, **CCTV should be put in place**.
11. Berkeley Homes are currently trying to change their planning permission around the South Quay Plaza area to remove the constraint for having the development car free. This will need **careful thinking about the southern approach** to the bridge – again keeping the cars and humans apart.

Local resident

12. I **fully support the proposal** for a new bridge linking Canary Wharf to South Quay. However I **object to the location of the proposed bridge**. The location of the proposed bridge will result in a level of **wind that will make it unsafe** for pedestrians.
13. I **suggest that the bridge is moved to the east** such that it aligns with the entrance to South Quay DLR and wind is blocked at both ends by buildings.
14. This would allow the existing **vehicle access to Discovery Dock and the Goodmans restaurant to be maintained**. The Bridge should form part of an **integrated transportation plan** for the South Quay/Marsh Wall area, that includes pedestrians, cyclists, emergency vehicles, services vehicles and cars/motorbikes as well as existing and planned parking provisions. The plan should fully

consider not only construction traffic volumes but the safety of pedestrians, including push chairs and children.

15. I have lived in the area for over ten years and am fully aware of how severe the winds and the gusts of wind can be on the proposed pedestrian access road. **There is a "wind tunnel" between the large buildings** that channels high winds through the narrow corridor that exists between the road outside Waitrose and Marsh Wall. Planting of trees on the south side will have negligible effects as the most windy season is winter when the trees will not have any foliage.
16. The existing pedestrian bridge suffers minor gusts, east/west across the bridge, however the buildings at both ends block winds in the north south direction. The high side to the bridge mitigates the **impact of the east/west gusts**.

Local resident

17. I note in the information about the plans that it mentions shared use of the bridge between pedestrians and cyclists, but no segregation between the two. I think this is a very bad idea.
18. As the current bridge has steps and a lift on the north side of South Quay cyclists are forced to dismount. The proposed **scheme would allow cyclists to ride across the bridge with their speed unabated**. There are enough cyclists already using the current bridge who seem to have little care for pedestrians, at **speeds that are incompatible with pedestrians**.
19. The worst users are the Deliveroo cyclists who race across the bridge to make their deliveries with little care for other uses. If you are crossing the bridge and **you can hear a cyclist behind you it is a very unnerving experience** and leaves you fearing a collision, and you rely on the cyclist to avoid you. It is made more difficult as you have to avoid people approaching you with the

cyclist coming up behind you. I've seen cyclists weaving in and out of pedestrians on the bridge and it makes me angry and fearful for their safety.

20. With the proposed design, with a straight approach on either sides of the quay, and with seemingly no measures to make cyclists slow down or even mitigate their speed **there is nothing there to prevent accidents between cyclists and pedestrians.** Not segregating the two introduces a risk of accidents which your plans do not seem to seek to mitigate. Your risk assessment for the bridge does not seem to **properly assess the risks and consequence.** I really think this is a poor decision and can't understand this approach.

Pan Peninsula Leaseholders & Residents Association

Pan Peninsula comprises some 760 flats (around 1,500 residents) – about half the current population in the Marsh Wall/South Quay area.

21. In short, **a bridge at South Quay is urgent and absolutely essential** given the imminent avalanche of new residents coming to the Marsh Wall/South Quay area. Without it, **the current 'Wilkinson Eyre' bridge further west will become impassable at peak times** as the new residents arrive. That bridge is also dangerous, with its sloping metal floor and lack of wind protection.
- Having said that, we urge you to improve the proposal as follows:
22. **The bridge should have solid side barriers,** strong and tall enough to **protect pedestrians and cyclists from the high winds** that can whip across the dock. The council's designers constantly fail to appreciate the strength of the wind gusts in this area, which will only get worse as more huge buildings along the dockside exacerbate the wind corridors, especially over the dock.

23. In your consultation presentation, you say that "the surface of the bridge deck will be designed to be smooth and quiet to walk on", and that it will be made of steel for lightness so it can open. You also seem to require the bridge to be sloping, as the north landing point is higher than the south one. This seems to repeat mistakes made in the existing bridge, which make it difficult and dangerous to walk on in bad weather. **The deck must be made of non-slip material, and it must be as level as possible, as well as having step-free access.**
24. You also say in your consultation presentation that **"the pedestrian and cycle paths will not be segregated as this would require a wider approach path than is available."** On the existing Wilkinson Eyre bridge cyclists (who often fail to dismount) are already a danger to pedestrians, and so **arrangements should be made for them to be segregated. If that means that the landowners on either end of the bridge have to provide a wider approach path, then so be it.** Let's not create a dangerous crossing for pedestrians on what you acknowledge "will become one of the busiest pedestrian bridges in London", with "70,000 - 80,000 pedestrians...and 2,500 - 4,500 cyclists... forecast to cross the South Dock every day by 2030".
25. With this very heavy volume of traffic over the new bridge, **encouraging each direction of people to keep to one side or the other is essential,** especially at peak times. Please therefore include clear 'keep left' or 'keep right' notices in your design.
26. We also note from your online survey that you are considering including a central observation platform/seating area on the bridge. Given the heavy volume of traffic at peak times, **any obstruction on the bridge would be counterproductive,** so we do not support that idea. This will be a major thoroughfare: not a tourist attraction.

London Cycling Campaign

27. This scheme is **supported with one significant concern**.
28. The consultation materials state “the bridge must be a minimum of 7.8m wide for pedestrians and cyclists to use comfortably... the pedestrian and cycle paths will not be divided as this would require a wider approach path than is available.” Likely flows of those cycling and walking here will be very high, with the consultation predicting over 70,000 pedestrians and over 2,500 cyclists using the bridge daily by 2030. **The bridge will also be located very close to one of TfL’s Top 25 highest potential cycling corridors** in its Strategic Cycling Analysis and in one of its highest growth areas, nearby to major planned cycling schemes such as the Canary Wharf – Rotherhithe bridge.
29. Given the above, it is difficult to see how 7.8m without any separation will be comfortable for anyone to use. It is also difficult to see why wider approach paths are not “available”. **The bridge should be wider, with wider approach paths, and with clear demarcation (using a raised strip such as found on Blackfriars Bridge) between walking and cycling areas.** Failure to do this would likely result in far lower cycling flows and amenity as pedestrians will otherwise dominate here, and similarly, would result in a far less comfortable environment with more conflict for those walking and cycling.

General points about cycling schemes:

30. **LCC requires schemes to be designed to accommodate growth in cycling.** Providing space for cycling is a more efficient use of road space than providing space for driving private motor vehicles, particularly for journeys of 5km or less. In terms of providing maximum efficiency for space and energy use, walking, cycling, then public transport are key.
-

Canary Wharf Group

31. As expressed previously, CWG is **supportive of the principle of a new pedestrian bridge**. You will be aware of comments we have made during the process and we will write to you separately on more detailed technical and property issues that need to be addressed. However, there is one particular issue that concerns us which we set out below.
32. It is proposed that the bridge be a key pedestrian route and a primary cycle route. The direct route off the northern side of the bridge through to Upper Bank Street into the heart of Canary Wharf is proposed to be between 10 Upper Bank Street and 50 Bank Street via a constrained area of public realm. This area is used by pedestrians and also occupants of the adjacent buildings as breakout space. It is also the key route to the children’s nursery and health centre located at promenade level on the south side of 10 Upper Bank Street.
33. As you are aware, Canary Wharf is home to a considerable number of workers and is visited by a vast number of people, the majority of which move around the area on foot. **Whilst we are supportive of cycling, in this case we do not consider it appropriate for the bridge and route through to Canary Wharf to be identified as a primary cycle route. There is simply not the space for pedestrians and cyclists to move safely** along this route at the same time and therefore you will appreciate our priority for this area has to be for pedestrians.
34. In the circumstances we would respectfully request that this element of the proposal be revised accordingly.
-

Canal & River Trust

35. **We welcome the recognition of the need for mooring points to remain available on South Dock wall.** The Trust does not want its ability to offer moorings in South Dock significantly adversely impacted by the development of a new bridge. **The existing South Quay mooring length is approximately 190 metres. The bridge would reduce large ship capacity on this quay to less than 100 metres.** We have previously discussed how the **provision of pontoons to the southwest of the bridge could provide compensation for it bisecting the existing deep-water mooring** and we remain of the view that these should be delivered. In addition, in commenting on the Tower Hamlets Local Plan, we have been consistent in requesting **protection for the active uses of the waterspace at Thames Quay, as Marsh Wall East and Marsh Wall West sites come forward for development.**
36. We note that the council state that the bridge will 'share the load of new crossing demand with the existing bridge'. In the interests of minimising the number of impediments to boat movements within the dock and delivering thriving, vibrant waterspaces, we suggest that **the number of bridge crossings should be limited.** We have previously suggested that the aim should be to remove the Wilkinson Eyre Bridge, if/when the new bridge is in place. If it has not already been designed with this in mind, **we would like to understand from the council what width the proposed new bridge would need to be for the council to be satisfied that only one bridge is required.**
37. We welcome the fact that the bridge is being designed to allow smaller boats to pass underneath when it is in the closed position. **A 3 metre clearance height when the bridge is in the closed position is sufficient from a navigation perspective.**
38. We are of the view that the **25m opening section should be clear and vertically unconstrained across its full width. If the bridge does not open perpendicular to the water, we suggest that some form of structure in the water to mark the edge of this channel will be required.**
39. We would want to see the bridge designed in such a way as to **appear relatively lightweight** and be of **high quality, robust and modern materials.** We would like the **abutments and piers to be of the minimum size possible** and preferably **tapered on the outside edges so as to appear smaller in section,** and provide as much open water beneath the structure as possible. We would like to see the bridge developed **perpendicular to the dock edge.** We look forward to seeing further details of the proposed design following this initial consultation.
40. In the interests of the structural integrity of the dock, we consider that the **foundations to the bridge should be separate from the dock wall.** We understand that this is consistent with your proposals.
41. The Trust will expect **maintenance and management responsibilities to be agreed as part of the agreement that will be required with us to deliver the bridge.**
42. Any works will need to **comply with the Trust's Code of Practice for Works Affecting the Canal & River Trust.**

GVA, on behalf of Berkeley Homes

43. Berkeley Homes has provided great support to enable the delivery of the South Dock Bridge and we continue to **consider it to be an important investment in infrastructure for the Isle of Dogs,** its residents and its visitors. As part of the approved South Quay Plaza development, a north-south pedestrian route has been carefully designed through the site to provide a safe transition from the proposed bridge and a legible route through to Marsh Wall, South Quay DLR

station and wider areas adjoining the site. The expanse of public realm at South Quay Plaza will also accommodate a range of spaces for seating, dwell, outdoor activities and child play space. These spaces will be enjoyed by residents and visitors to the Isle of Dogs.

44. In addition, the Section 106 Agreement secured pursuant to the South Quay Plaza development safeguards a number of areas along the South Dock frontage which has enabled multiple options for a bridge landing zone to be explored by the LBTH and its partners.

45. Whilst there are a **number of aspects of the proposed South Quay Bridge that are welcomed, including the proposed bridge location and landing zones and the provision of access to wheelchair users**, there are a number of aspects that raise serious concerns. These **concerns relate to the safety, quality, functionality and management of the public realm being delivered as part of the South Quay Plaza development.**

46. ...the approved public realm caters for origin and destination cycle trips expected as part of the estate wide day-to-day operation, rather than as a thoroughfare for cyclists between Marsh Wall and Canary Wharf. As such, **the volume of cycle journeys assumed as a result of the bridge and resultant traffic has not been foreseen.**

47. From design conception there has been an overarching objective to prioritise spaces for play and amenity and this has been achieved through strategic surface alignment and delineation. To accommodate the volume of cyclists that are expected to use the bridge, the consultation materials states that it is intended to be designed with a **minimum 7.8m width. In a scenario where this width is maintained from the bridge and through the South Quay Plaza site, this would have a detrimental impact on the approved and committed public realm due to be delivered as part of the South Quay Plaza development.** The amendments that

would be necessary to provide a safe shared passageway for both pedestrians and cyclists would ultimately result in the **significant loss of public open space and child play space.** The public realm proposals at South Quay Plaza have been considered acceptable by both the LBTH and the Greater London Authority, both in terms of quality and ensuring that the impacts of the proposed development in terms of providing adequate access, public open space and child play space are being appropriately mitigated. To allow a shared surface of the nature currently envisaged in the consultation material, **the resultant public realm would unlikely provide sufficient open space and child play space to appropriately serve the South Quay Plaza development and therefore the quality of the public realm would be reduced considerably**

48. The provision of a **free flowing shared pedestrian and cycle route** through the site which may be necessary to support the proposed capacity of the bridge **would result in significant risk of safety to pedestrians, visitors and children utilising the public realm at South Quay Plaza.** It is also noted that the current **public realm and cycle infrastructure located at both the southern and northern ends of the proposed bridge, and the wider connecting areas, do not support the provision of up to 4,500 free flowing cycle journeys a day.** We consider that a shared bridge with steady paced pedestrians commuting to work, the predicted level of cycling activity, alongside the forecast volumes of pedestrian flow, generates the **likelihood for unsteadiness and collision.** This risk is not just limited to the bridge itself but extends to the pavements and public realm that approach the entry and exit points at each end of the bridge.

49. On behalf of Berkeley Homes, we raise **significant concerns with the proposal to create a shared pedestrian and cycle bridge** in terms of **lack of a wider plan to ensure that sufficient infrastructure, public realm and highway provisions are in place** to ensure the bridge

could be delivered, operated and managed effectively and safely. For these reasons, we **strongly suggest that the proposals are reconsidered and redesigned to prioritise pedestrians.** Further, that the **bridge is designed to dissuade cyclists from using it**, with alternative routes provided in order to ensure the safety and enjoyment of those using the public realm and various pedestrian accesses stemming from the bridge.

Tower Hamlets Wheelers

Tower Hamlets Wheelers is the borough group of the London Cycling Campaign with 430 paid-up Members and a further 864 registered supporters living in Tower Hamlets.

50. We **strongly support the building of a new walking and cycling bridge over South Dock** in principle, although we have a **significant concern about the proposed design with regard to the lack separation between pedestrians and cyclists.**
51. South Dock and the Canary Wharf estate have historically formed a barrier which has inhibited walking and cycling journeys between the Isle of Dogs and the rest of Tower Hamlets, especially given that the A2106 either side of South Dock (Westferry Rd/Preston's Rd) is rather inhospitable to people walking and cycling. We hope and anticipate that the proposed bridge will help to reduce this severance effect.
52. TfL's Strategic Cycling Analysis (June 2017) has identified three north-south routes from the Isle of Dogs as connections with significant cycling potential. We note that the new bridge could form part of the future implementation of one of these identified connections.
53. We also note that the Strategic Cycling Analysis identifies the areas immediately to the south and north of the proposed bridge as areas of highest cycling demand and growth.
54. We therefore **ask that plans for high-quality onwards cycling routes are progressed alongside the plan for this bridge, including through the Canary Wharf estate itself and through the Isle of Dogs.**
55. We are concerned that the consultation document states that "[t]he pedestrian and cycle paths will not be segregated as this would require a wider approach path than is available."
56. It is not clear to us why the approach paths on either side of the proposed bridge site are not considered wide enough. Photographs of both north and south quays are provided overleaf, and both appear clearly wide enough to accommodate both a separated cycleway and walkway.
57. In addition, in early stages of its existence at least it is likely that the bridge will be used by more pedestrians than cyclists. **At peak times, therefore, it is likely that (without separation) pedestrians will spread across the bridge deck sufficiently widely that it will not be possible to cycle through.** Even at off-peak times, there is risk of pedestrian-cyclist conflict if the two modes are not separated.
58. **Separation could be in the form of a simple raised demarcation kerb** (pictured right) with **different surface colouring for pedestrians and cyclists.**
59. We therefore **ask that the borough look again at the question of separation between cyclists and pedestrians** on the closed bridge, and redesign the approaches such that it is possible.
60. We also ask that whether pedestrians and cyclists are separated or not, the **bridge is made as wide as feasibly possible in order to provide plenty of space for both modes.**
61. In order to be accessible to all cycling users, the **bridge should have a shallow gradient (no more than 1 in 20).** Given the anticipated low profile of the bridge, this should not be difficult to achieve.

62. To **enable all types of bicycles to comfortably cross, including cargo bikes and disability-adapted bikes/trikes, the bridge should not have any types of barriers.**

5.12 SUMMARY OF KEY MESSAGES FROM LBTH MEETINGS

In addition to consultation session with the key stakeholders for the project, presentations were given and meetings held with LBTH stakeholders, including Members, - local Ward Councillors and Portfolio Holders; the Tower Hamlets Accessible Transport Forum. The key messages from these meetings are set out below.

MEMBERS BRIEFING

Cycling

- Cycling demand may be lower than projected
- Cyclists are more likely to use Manchester Road and Wood Wharf to travel to and from Canary Wharf, as they will be easier routes to use.
- Wood Wharf is not designed as a cycle route but cyclists will use it.
- Current routes are not safe for cyclists – consider wider improvements to routes.
- Cyclists and pedestrians using the same space on the bridge could be problematic.

Bridge options

- The number of pedestrians forecast for the existing Wilkinson Eyre Bridge in the presentation may be slightly different.
- Do not take away the existing bridge.
- Consider a floating footbridge east of South Quay (as set out in early Isle of Dogs Opportunity Area Planning Framework).
- Consider a third footbridge across Heron Quay (option for the future).

South Dock Bridge design & public realm

- Draw on the industrial heritage and working class roots of the area.
- Have a bridge that relates to the history of the area rather than a glitzy steel bridge.
- An example could be a bridge designed in harmony with the dock cranes.
- Look at the Blue Bridge by the London Legacy Development Corporation as an example of a bridge which links to the history of the area.
- The Excel Bridge is another good example.
- Engage with the Friends of Island History Trust on local history.
- The Gillard Developments scheme at Millharbour demonstrates good public realm.
- Recognise that the South Dock Bridge will be a landmark structure.
- Consider an opening that will allow larger size vessels to get through such as luxury yachts.
- Beware of delivery mopeds when designing the bridge.

Stakeholders

- Speak to Berkeley Homes about their construction management plan for South Quay Plaza to ensure that it links up with the construction plan for South Dock Bridge.
- The existing bridge is maintained by CWG as it's not well maintained by CRT and CWG consider it as a gateway to Canary Wharf.
- Link with the CWG's emerging Water Space Strategy. The strategy looks at enabling the public to access the dock edge – South Dock Bridge could have a role there.
- Agreeing maintenance for the bridge will be the most important part of delivering it.
- Clearly define land ownership and the roles of different stakeholders /landowners.

TOWER HAMLETS ACCESSIBLE TRANSPORT FORUM MEETING

- Design a flatter bridge than the existing bridge so it's easier to access for people with mobility issues (the current bridge is too steep).
- Consider delivering other bridges in the area to improve connections.
- Deliver improvements to wider area and make the area more accessible/walkable.
- The waterway along the south dockside should not be promoted as a cycle route as its too narrow.
- Pedestrians and cyclists should ideally be segregated on the bridge for safety – this is problematic with the existing bridge.
- The bridge could be named after a notable local person – such as Jack Dash.
- Could work with local schools on naming the bridge

SOUTH DOCK BRIDGE PROJECT BOARD MEETING

The South Dock Bridge Project Board made a number of comments on the conceptual designs. This included assuring the bridge is Disability Discrimination Act (DDA) compliant, including lighting within the bridge design and including mitigation measures for hostile vehicles. It was also noted that the project costs should include the cost for building on other people's land.

5.13 SUMMARY OF KEY MESSAGES FROM STAKEHOLDER LIAISON

The project team engaged with key stakeholders to inform the design development process. This section summarises that engagement, which is ongoing. Meeting minutes are attached in Appendix C.

London Borough of Tower Hamlets

The LBTH commissioned this study. The project team presented to the South Dock Bridge Project Board (January 2018), officers and Members (February 2018) to receive feedback on the emerging design.

Transport for London

TfL are a strategic partner in the South Dock Bridge project.

- TfL support the principle and design of South Dock Bridge
- They were concerned about the lack of information about integration of pedestrians and cyclist links into the wider route network
- They asked that key principles be highlighted in the report to help ensure a successful outcome, and should include:
 - 1 Span architecture / structure:
 - Main span
 - Lifting mechanism
 - 2 Landings
 - Townscape
 - Landscape and public realm -integration of landing points
 - 3 User experience:
 - Inclusive design
 - Personal safety / crime and disorder
 - 4 Onward journey design principles
 - 5 Environment and sustainability
- TfL recommended an absolute minimum cycling width of 1.8m for the bridge
- TfL expressed a concern over the proposed interface with the fire stairs in Option B (although agreed this was a neater solution) and asked if alternative solutions had been explored. A clear reason for this arrangement should be provided in the report.

- Concern was raised over Option B and the barrier created by the fire escape steps and guardrailing, and the bulk of the counterweight which may block the line of site across the bridge. This should be acknowledged through design development.
- TfL expressed support for the automatic drop down barriers, and requested solutions for concealing the barriers when not in use.
- The bridge deck surface should provide a comfortable and safe surface for cyclists.
- Further consideration should be given to seating to ensure it does not disrupt pedestrian and cyclist flow by taking away space.
- TfL expressed reservations about the significant visual impact of the counterweight element of the bascule bridge in the Rotterdam example, and wanted to understand if there's an opportunity to make this less visually imposing. They do however appreciate the need for the bridge to have visibility/presence from the estate.
- Concern was raised about the negative design implications of a safety fence around the counterweight.
- TfL support the need for the bridge to become an exemplar for the principles set out in the new Mayor's Transport Strategy
- TfL proposed that a cycle trial be undertaken to understand how the bridge will work.
- TfL welcomes the Upper Bank Street - oversailing bridge study, but have asked for more detail about the south interface and justification for this design.
- TfL would like further consideration given to the integration of the bridge on the northern side to take advantage of its south-facing space with views of the water, by providing an active and usable space. TfL offered their Urban Design Team to assist with this.
- TfL queried whether the conceptual design should go into further detail about the operation of the bridge.

Canal & River Trust

The CRT own and manage South Dock itself, as well as the immediate dock walls where the bridge would land.

- CRT supports the development of a bridge on the Upper Bank Street alignment
- CRT require a 15m x 3m permanent navigable channel, and a 25m channel when the bridge is open, which needs to be unrestricted in its height. They are happy for these channels to overlap or be separate
- CRT expressed a preference for bridge Option B due to the navigable channel being located to the north of South Dock, which allows more space for pontoon moorings to the south of the Dock
- The existing navigable channel in South Dock is 30m wide. Any change to the navigable channels in South Dock would require an amendment to the designated Infrastructure Trust Property channel, with Department for Environment, Food and Rural Affairs approval

Canary Wharf Group

CWG own the land to the north of South Dock, including the landscaping on the northern approach to the Upper Bank Street alignment.

- CWG supports the development of a bridge on the Upper Bank Street alignment
- CWG's preference is for cyclists to dismount when they cross South Dock Bridge, and for it to be used for local cycle access only, i.e. not to form part of the strategic cycle network
- CWG agree to the principle of modifying the landscaping on the northern approach, with the costs being met by the South Dock Bridge project. The removal of existing trees must be minimised and the same number of trees should be provided in the modified scheme as at present
- The emergency escape steps from the basement must be retained, but changes to the balustrading around them may be considered

- The bridge must:
 1. not apply any load to the dock wall,
 2. not block basement ventilation systems,
 3. retain function of existing emergency escape steps.

Berkeley Homes

Berkeley Homes own the land on the southern side of South Dock, where they are developing the South Quay Plaza scheme. During this phase of the bridge development (November 2017-February 2018), the first two buildings of South Quay Plaza had begun construction – foundation work and initial storeys.

- Berkeley Homes supports the development of a bridge on the Upper Bank Street alignment
- Berkeley Homes expressed a preference for bridge Option B due to the lifting machinery being located on the northern side of South Dock and the southern landing therefore being narrower than Option A
- They were concerned about the anticipated volume of pedestrians and cyclists crossing the bridge and their land. It is their preference to minimise the volumes of pedestrians and cyclists who are routed through the South Quay Plaza scheme
- Discussions are ongoing regarding a suitable landscaping arrangement to accommodate pedestrians and cyclists through South Quay Plaza. This depends upon the proposed delivery of a link with the basement of Discovery Dock East (neighbouring building to the west of South Quay Plaza). This link would allow the removal of existing car lifts and therefore the requirement for motor vehicle access along the north / south access route to the Upper Bank Street alignment. If the link cannot proceed, motor vehicle access would need to be retained through South Quay Plaza's landscaping. Berkeley Homes are engaging with residents of Discovery Dock East regarding this basement link.

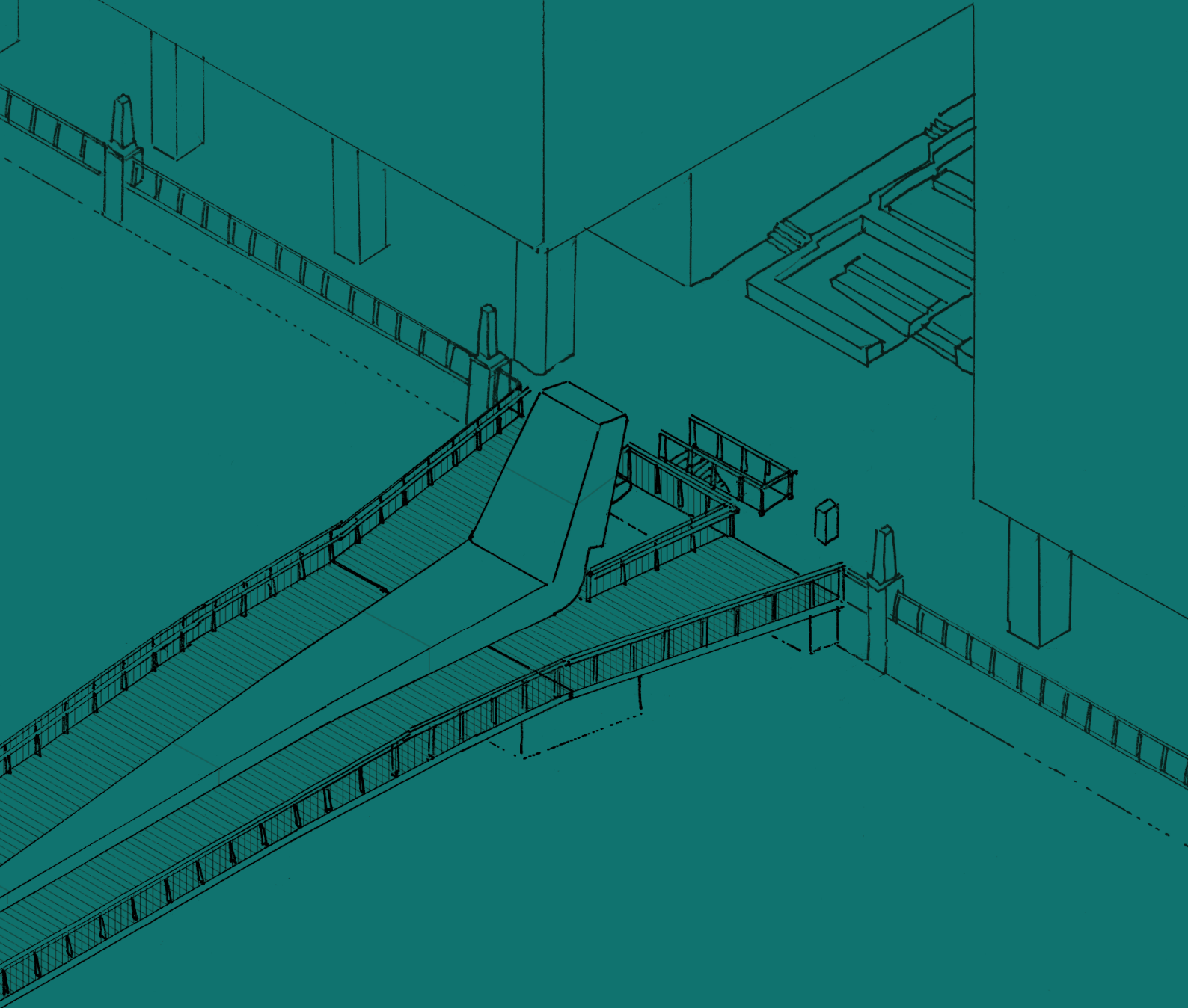
Dockland Scout Project

- The Dockland Scout Project are concerned that the proposed bridge will impact their access to the waterway and would significantly hinder their activities as sea scouts.
 - Moving the bridge alignment further along the dock (adjacent to the mobile food stands and in between the existing and proposed bridge) would allow them to better access the waterway.
 - LBTH suggested consideration could be had of opening the bridge for certain periods during the weekends as a potential mitigation measure.
 - The Sea Scouts do heavily use the dock waters over the weekend which is likely to be outside peak use times for the bridge, but do not think that opening the bridge for the Sea Scouts to carry out their activities at certain times on the weekend would be suitable mitigation.
 - Concern was raised as to how enforceable this would be as it is not yet known who will be responsible for operating the bridge. It was suggested this could be detailed in a service level agreement between the council and the agreed operator.
 - A close bridge clearance height of 3.0m above the waterline would be insufficient for accommodating the taller sailing vessels used by the Sea Scouts.
 - Using the Thames as an alternative location for the Dockland Scout Project would not be viable because of the cost incurred by the CRT of using the locks to gain access.
 - Relocating the Dockland Scout Project to Millwall Dock was discussed; however submerged pipes presented a problem for the Sea Scouts larger vessel relocating via the Millwall Cutting.
 - In addition, it is difficult to open the Millwall Cutting Bridge as it is only opened by CRT infrequently.
 - The Dockland Scout Project intends to oppose the bridge at planning application stage throughout the whole delivery process.
- The Dockland Scout Project said that they have support from MP Jim Fitzpatrick, the media and other local groups.
 - Concern that Dockland Scout Project will need to relocate their activities and sell their building if the bridge goes ahead.
 - The Dockland Scout Project have been encouraged to make a written representation to the council in response to the public consultation, specifically outlining their concerns and detailing their hours of operation and any suggestions they might have on the design.

6 KEY MESSAGES SUMMARY

- 6.1 INTRODUCTION
- 6.2 MOVEMENT AND ACCESS
- 6.3 CONSTRAINTS AND OPERATION
- 6.4 DESIGN AND AESTHETICS
- 6.5 SAFETY AND SECURITY
- 6.6 PROCESS AND ENGAGEMENT
- 6.7 NEXT STEPS





SOUTH DOCK BRIDGE

Consultation Report
May 2018

6.1 INTRODUCTION

Overall the vast majority of respondents said they supported delivery of the bridge in principle (96%) and said they would use the bridge once it was delivered (96%). A number of key issues were raised for consideration at the next stage of the design process, and these can help inform the brief for the next stage of the project. These key issues are identified below.

6.2 MOVEMENT AND ACCESS

Combined pedestrian and cycle use

This was by far the greatest issue during consultation, with many respondents stating that pedestrian and cycle use should be segregated, due to safety concerns about collisions. Suggestions for accommodating this included widening the bridge itself and the north and south approaches.

Some respondents, including strategic stakeholders, stated that cyclists should not be able to use the bridge because of the high flows predicted and potential movement clashes. There were conflicting anecdotal views on whether the cycle route would be picked up by cyclists as a strategic route or not. Some felt that Manchester Road would be used instead while others considered this route inadequate and easily usurped.

It should be noted that concern regarding collisions related to experience on the current bridge. Whilst the consultation material stated that the proposed bridge would be 7.2m of usable space, it did not compare this with the 3.6m width of the existing bridge.

Of respondents who completed the feedback form, 61% said they would use the bridge on foot, 14% by bike and 22% by both. The width of the bridge was the second highest priority for people who identified these on the feedback form, voted for by 96 people. Access by bicycle was identified as a priority by 66 people. This was a 'middling' score as a priority.

This issue should be considered in detail at the next design stage, with TfL's strategic objectives; stakeholders' concerns; and public consultation feedback all taken into account. Further stakeholder discussions will be required, along with review of the predicted use levels and the wider movement network.

6.3 CONSTRAINTS AND OPERATION

Approaches from north and south

Respondents stressed that the approaches from the north and south should be spacious and free of obstacles. The approach from the north may require a reworking of the current landscaping, to provide a fully accessible approach that can accommodate flows

The approach from the south could present a challenge in terms of pedestrian/cycle movement flows, South Quay Plaza play areas, and Discovery Dock vehicular access.

These should be reviewed in design terms and in collaboration with key stakeholders at the next design stage.

Wider movement issues

Respondents flagged that cycle and pedestrian routes should integrate well with the wider network. Conflicting opinions were provided on whether cycle routes south of the bridge should run south through the South Quay Plaza scheme or along the dockside, with concerns about each of these being narrow or having obstacles. This should be explored at the next stage, in conjunction with the balance of cycle and pedestrian use of the bridge.

Seating on the bridge

A small majority of feedback from respondents stated that they do not think seating on the bridge would add value and would likely hinder the movement of pedestrians and cyclists. Some respondents suggested seating might be appropriate if it explicitly does not remove movement capacity, and this should be explored at the next stage of design.

Maintaining dock access for boats

A number of respondents referenced the requirement to maintain access for the dock area for leisure activities, including the Sea Scouts weekend dinghy activities. This would require an 8m high bridge, or for the bridge mechanism to be open for a period of time while the scheduled activities take place. This should be reviewed at the next design stage with key stakeholders, to explore the operational issues. In particular, a stakeholder discussion should take place with the Sea Scouts during the next stage of the design process, to explore their use patterns of the dock and requirements.

Requests were also made to ensure that larger vessels such as luxury yachts can access the dock when the bridge mechanism is open.

Floating pontoons to moor boats

The CRT highlighted that pontoons for boats to moor in the dock should be provided on the south side of the dock. This has been reviewed in principle but not in terms of detailed design, and should be explored at the next stage.

The bridge should be swift to open and close

This was raised by respondents and Members as an issue with the existing bridge that should not be repeated on the new crossing.

Maintenance and bridge operation

Early agreement on maintenance and bridge operation was sought by the CRT and raised by Members as a key issue. This should be explored by the LBTH team and TfL in collaboration with CRT and other key stakeholders.

East-west wind conditions

Wind conditions for the bridge, and particularly the potential impact of east-west cross winds was raised by a number of people in written correspondence. Some respondents suggested that the bridge should have solid sides to provide a comfortable environment, whilst others were

concerned that this would be a visual barrier on the dock. Possible design options for mitigating the impact of east-west winds, the technical constraints for these, and the impact they might have should be explored at the next stage of the design. The pros and cons of providing solid walls to the bridge should be reviewed.

Bridge foundations

The CRT highlighted that the foundations to the bridge will need to be separate from the dock wall. This was a known constraint for the Stage 2 designs and has been taken into account, but must be retained as a design constraint during the next stage.

6.4 DESIGN AND AESTHETICS

Design is a priority

Architectural design, materials and appearance was identified as the highest priority for people who identified these on the feedback form. 97 people suggested it was a priority for them. Members' feedback suggested the design process should recognise that the South Dock Bridge will be a landmark structure.

Simple and elegant approach

Most respondents stated that they would like the bridge to be elegant, unobtrusive and contemporary, with a neutral finish. This was reported in the material choices stated in the feedback form and in the reasons provided for the bridge preference in the form. A couple of written responses also mention the need for the bridge to appear lightweight. One respondent did suggest that a solid and robust design will hold its own against the surrounding buildings and architecture at South Dock. These comments should be used to inform the brief for the next stage of design for the bridge.

Draw on industrial heritage

Members and some respondents suggested that the area's industrial heritage, dock cranes and working class roots should inform the bridge design. Suggested examples included the LLDC Blue Bridge, the Excel Bridge and the Millharbour public realm.

It was suggested that the design team engage with the Friends of Island History Trust on local history, to inform the design development.

Quiet bridge surface

A high priority for respondents is a quiet bridge surface, with references made to the existing bridge failing in this regard. It was the third highest priority identified by respondents on the feedback form (89 people voted for it) and was raised in additional comments and written correspondence.

6.5 SAFETY AND SECURITY

The bridge should be well lit in the evening

A few respondents raised the fact that the bridge and its immediate surrounds should be well lit in the evening and on winter afternoons, to ensure that users feel safe and comfortable. A lighting strategy has not yet been explored and this should be included in the next stage of design.

Barriers to prevent inappropriate vehicle use

A few respondents and Members raised concerns regarding delivery mopeds and their illegal use of the bridge. Staggered barriers were suggested to mitigate against this though other respondents suggested there should be no barriers, to ensure that the bridge is accessible and inclusive for all users. Easy access to the bridge by ramp and inclusivity for all users both scored highly on respondents identified priorities for the bridge, with 82 and 78 votes respectively. This will need to be carefully considered during the detailed design phase.

Unwanted congregating on the bridge

A few respondents raised concerns about the potential for groups of people to congregate on the bridge, which could be intimidating, and for 'pan-handlers' to set up on the bridge. One person raised the issue of this being potentially dangerous if cyclists use the bridge.

Compare width to other well known bridges

In discussing the bridge width with consultees at events, it was agreed that it would be useful to provide information on the widths of other well known bridges in London, so that a tangible comparison can be made. Suggestions include the existing South Dock bridge and the Millennium Bridge.

6.6 PROCESS AND ENGAGEMENT

Co-ordinating with Stakeholder strategies

A number of relevant strategies were raised by stakeholders, which should be considered as designs develop during the next stage of the project. These include but are not limited to:

1. The South Quay Plaza Construction Management Plan
2. CWG's emerging Water Space Strategy
3. CRT's Code of Practice for Works Affecting the CRT

Naming the bridge

It was suggested by the Accessible Transport Forum that the bridge could be named after a notable local person – such as Jack Dash. The group offered to provide a list of suggested names. It was also suggested that LBTH could work with local schools on naming the bridge

Advertising consultation

One person suggested that future consultation events could be advertised using the leaflet stands in Canary Wharf shopping mall.

Speak with additional stakeholders

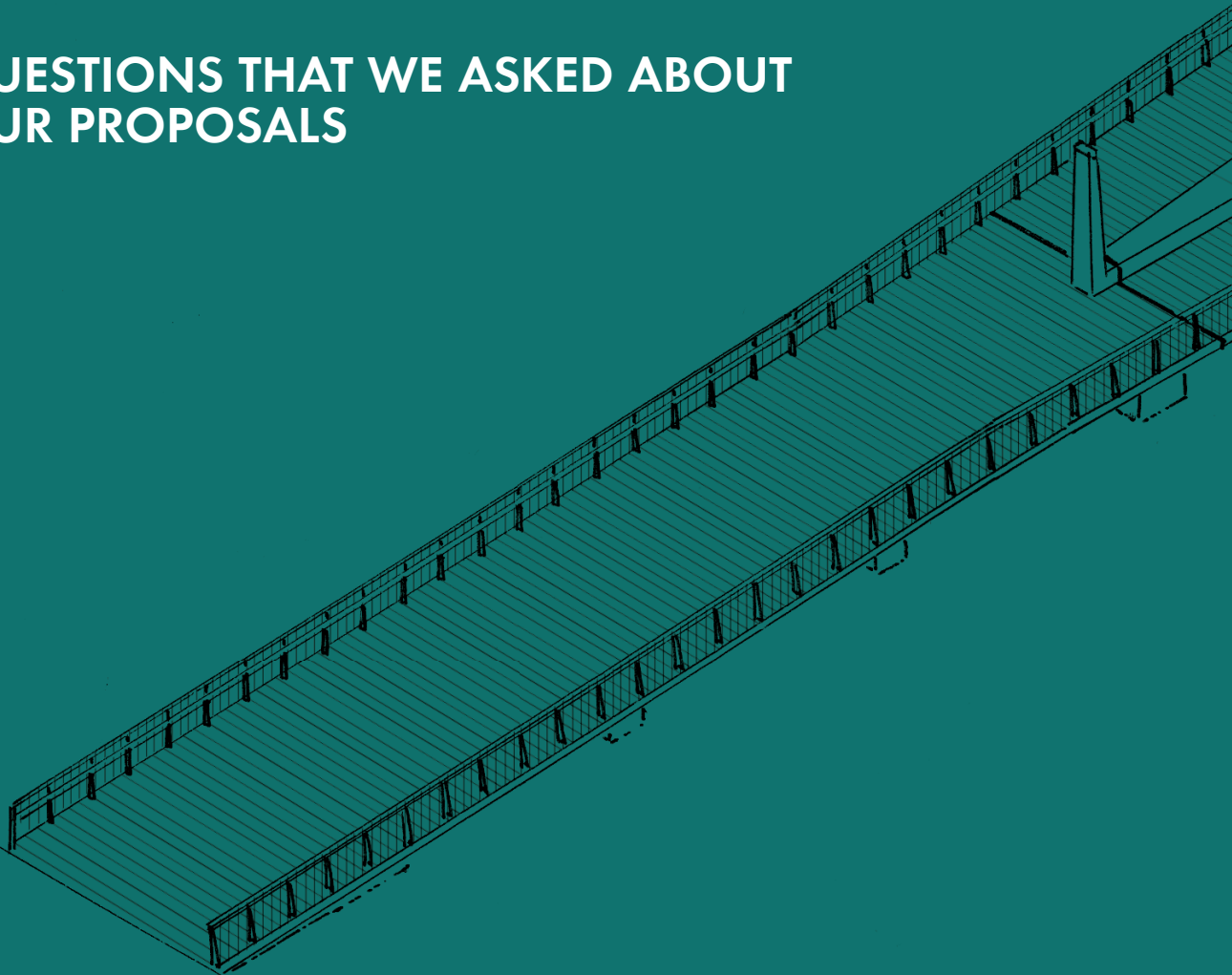
The Sea Scouts were identified as an additional stakeholder during consultation and discussions with the group will be ongoing. A review will be undertaken of other potential stakeholders.

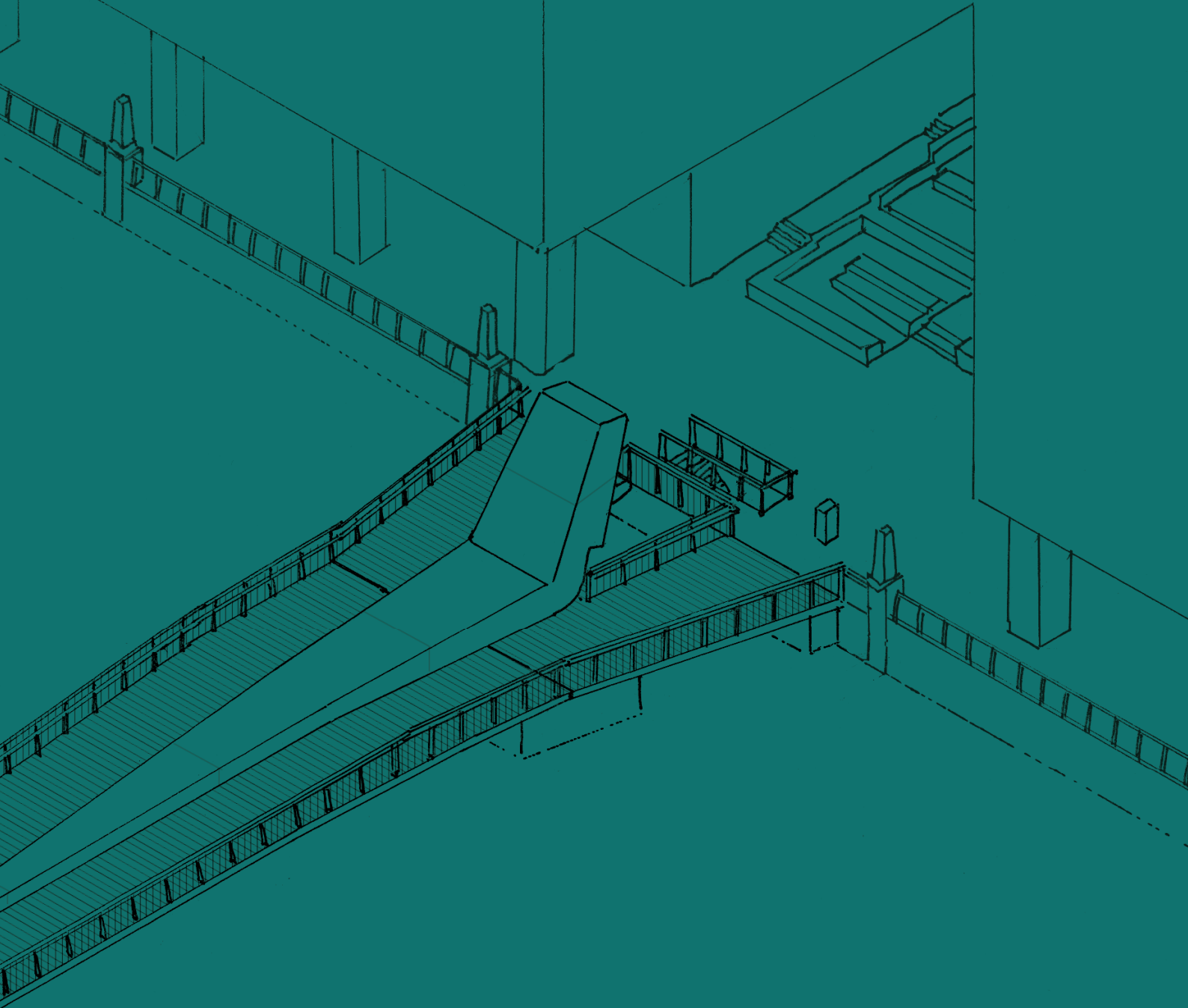
6.7 NEXT STEPS

The results from the public and stakeholder consultation set out in this report will contribute to the brief for the next stage of the design process. This will enable feedback to directly inform the evolution of the design as a planning application is prepared.

APPENDIX A

QUESTIONS THAT WE ASKED ABOUT
OUR PROPOSALS





SOUTH DOCK BRIDGE

Consultation Report
May 2018

QUESTIONS THAT WE ASKED ABOUT OUR PROPOSALS

These pages show the consultation feedback form distributed to those who attended the consultation events.

SOUTH DOCK BRIDGE CONSULTATION FEEDBACK FORM



Thank you for viewing the exhibition on the proposed pedestrian and cycle bridge linking Canary Wharf and the Isle of Dogs. Please take a moment to complete the feedback form. Your feedback will inform detailed design and the planning application later this year. Please drop your form in the box or return to: Infrastructure Planning Team, Tower Hamlets Council, Town Hall, 2nd Floor, Mulberry Place, 5 Clove Crescent, E14 2BG. You can also respond online at www.towerhamlets.gov.uk/southdockbridge

- 1.** Do you support the introduction of a new pedestrian/cycle bridge over South Dock in principle? *Please circle.* **YES / NO**

- 2.** A. Are you likely to use the new bridge? *Please circle* **YES / NO**
 B. Where would your journey start and finish? **From To**
 C. How often would this be? *Please circle* **Daily | Weekly | Less frequently**
 D. Would this be by bike or on foot? *Please circle* **Bike | Foot | Both**

- 3.** It is likely that the bridge will need to be constructed from steel, but options exist for the surface finish. Which of the following treatments do you prefer? *Please tick*

Raw steel	<input type="checkbox"/>
Neutral tone	<input type="checkbox"/>
Bright tone	<input type="checkbox"/>
Dark tone	<input type="checkbox"/>

- 4.** A. Which of the example bridges shown on the website/in the exhibition do you prefer? *Please tick*

Denmark	<input type="checkbox"/>
Netherlands	<input type="checkbox"/>
New Zealand	<input type="checkbox"/>

B. Why is this?

- 5.** There could be scope for creating places to sit on the bridge, or a central viewing platform. Do you think this would be a valuable addition to the bridge? *Please circle* **YES / NO**

PTO

6. Which of the following aspects are most important to you for the bridge? *Please tick as appropriate*

- Easy access to the bridge deck by ramp
- Access by bicycle
- Accessibility and inclusivity for all types of user
- Architectural design, appearance and materials
- Urban realm and landscaping around the bridge landing sites
- Incorporation of public art
- Width of the bridge deck
- A quiet bridge surface
- A quick mechanism for lifting/lowering the bridge for boats
- Construction impacts
- Environmental impacts
- Onward journey connections
- Operation and maintenance of the bridge
- Safety and security

Other, please specify

ABOUT YOU

Do you live, work or socialise locally to the proposed bridge?
Please tick as appropriate

- Live
- Work
- Socialise

Are you male or female? *Please tick*

- Female
- Male

What age bracket do you fall into? *Please tick* 0-24 25-40 41-54 55-70 71+

What is your home postcode?

What is your ethnicity?

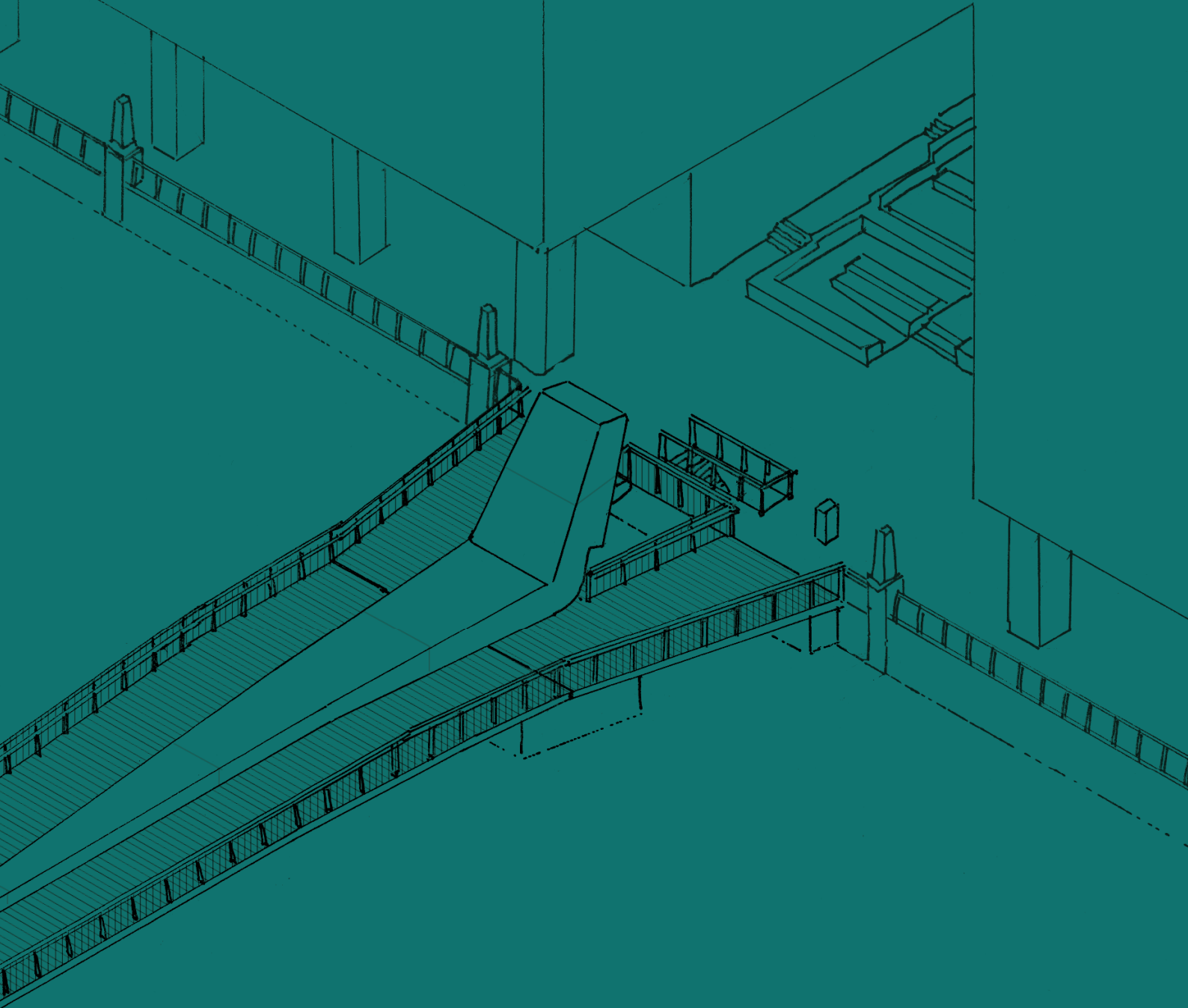
THANK YOU!



APPENDIX B

COPY OF LEAFLETS / POSTERS





SOUTH DOCK BRIDGE

Consultation Report
May 2018

COPY OF LEAFLETS AND POSTERS

The following pages present copies of the leaflets and posters used to promote the consultation period and events for South Dock Bridge.

The posters were displayed at:

- Council buildings
- Local libraries
- Other public venues in the area

The leaflets were distributed to:

- Local libraries
- Council buildings
- Local businesses and other local venues in the area (including Hazev restaurant and the Island Health Centre)

Leaflets were also distributed at:

- The existing bridge
- South Quay DLR station
- Heron Quay DLR station
- Other locations in the surrounding area

SOUTH DOCK BRIDGE

EXHIBITION AND CONSULTATION



A new pedestrian and cycle bridge is proposed to link South Quay and Canary Wharf in the Isle of Dogs. Information on the plans can be found online, along with options to comment.

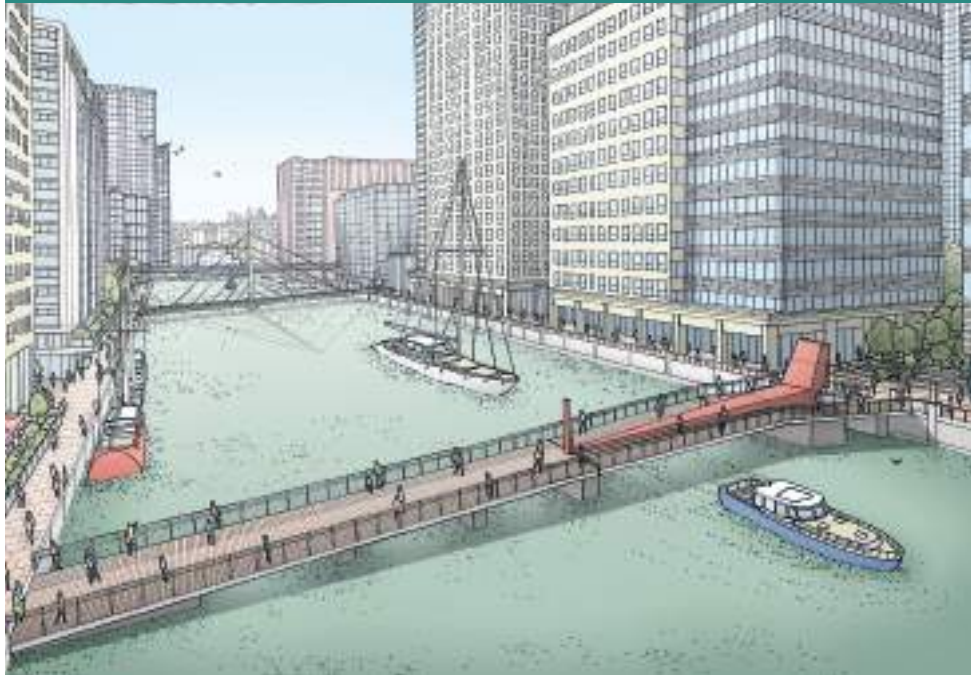
www.towerhamlets.gov.uk/southdockbridge

Alternatively, please come along to the exhibition to see plans for the bridge, speak to the team, and give your feedback. You can email comments to: Infrastructure.Planning@towerhamlets.gov.uk

Tuesday 27 February 2018, 11am-3pm
West Winter Garden,
35 Bank Street E14 5NW

Saturday 3 March 2018, 10am-1pm
Alpha Grove Community Centre,
Isle of Dogs E14 8LH

Thursday 8 March 2018, 5pm-8pm
Canary Wharf Idea Store,
Churchill Place E14 5RB



A5 Printed leaflet

SOUTH DOCK BRIDGE

Find out about the proposed new bridge to link South Quay to Canary Wharf in the Isle of Dogs.

Exhibition:

Tuesday 27 February, 11am-3pm
West Winter Garden
E14 5NW

Saturday 3 March, 10am-1pm
Alpha Grove Community Centre
E14 8LH

Thursday 8 March, 5pm-8pm
Canary Wharf Idea Store
E14 5RB



Find out more and comment:

www.towerhamlets.gov.uk/southdockbridge

Infrastructure.Planning@towerhamlets.gov.uk



SOUTH DOCK BRIDGE

EXHIBITION AND CONSULTATION

A new pedestrian and cycle bridge is proposed to link South Quay and Canary Wharf in the Isle of Dogs. Information on the plans can be found online, along with options to comment.

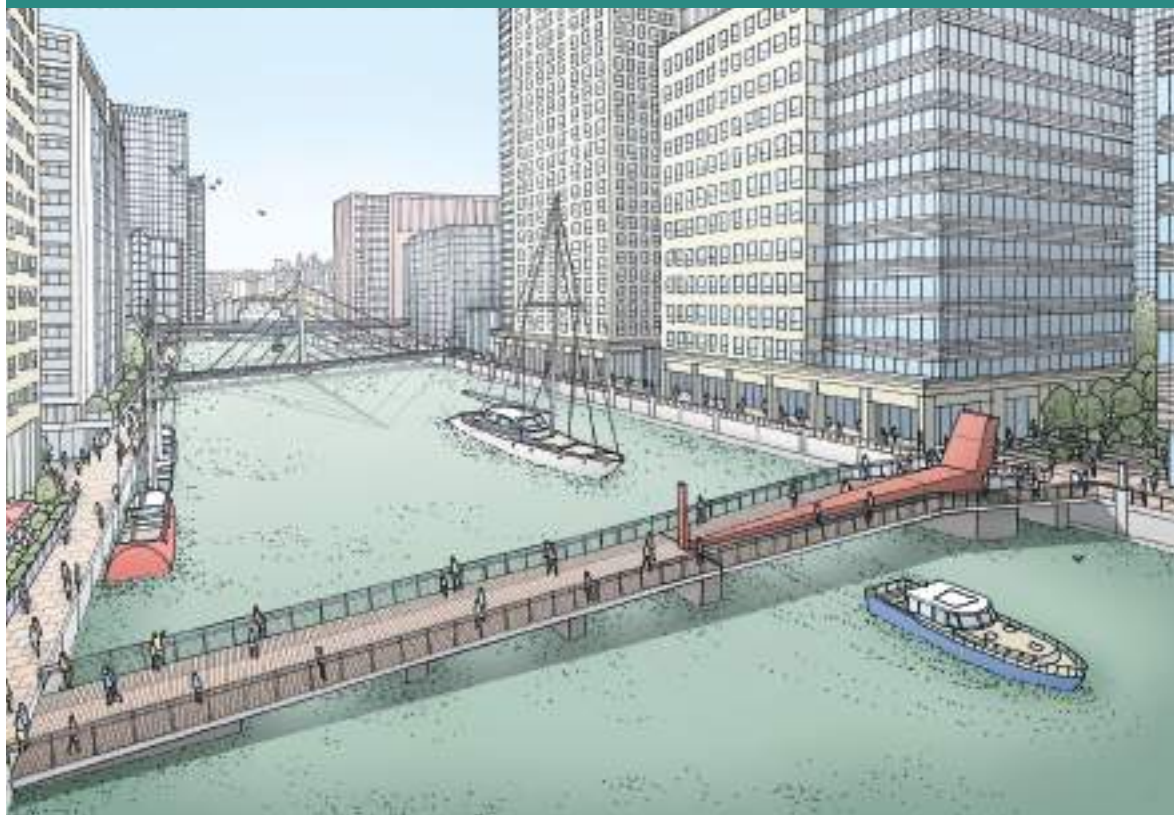
www.towerhamlets.gov.uk/southdockbridge

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Alpha Grove Community Centre,
Isle of Dogs E14 8LH

Thursday 8 March 2018, 5pm-8pm
Canary Wharf Idea Store,
Churchill Place E14 5RB



A2 Printed poster

BOARD 1

SOUTH DOCK BRIDGE

PUBLIC CONSULTATION

welcome

Welcome to the public consultation on a proposed new pedestrian and cycle bridge to connect Canary Wharf and the Isle of Dogs, called the South Dock Bridge.

An earlier study has shown that the bridge should align with Upper Bank Street on the north bank of the South Dock, and the South Quay Plaza scheme on the south bank. The location can be seen on the aerial view to the right.



Location of South Dock Bridge from the south (shown in red)

how to comment

We welcome your comments and suggestions; a six week consultation period is being held from **Monday 12th February 2018 to Friday 23rd March 2018**.

Consultation responses can be made in a number of ways:

by post:

Return your completed response form to:
Infrastructure Planning Team
 Tower Hamlets Council
 Town Hall
 2nd floor, Mulberry Place
 5 Clove Crescent
 E14 2BG

online:

www.towerhamlets.gov.uk/southdockbridge

by email:

Infrastructure.Planning@towerhamlets.gov.uk

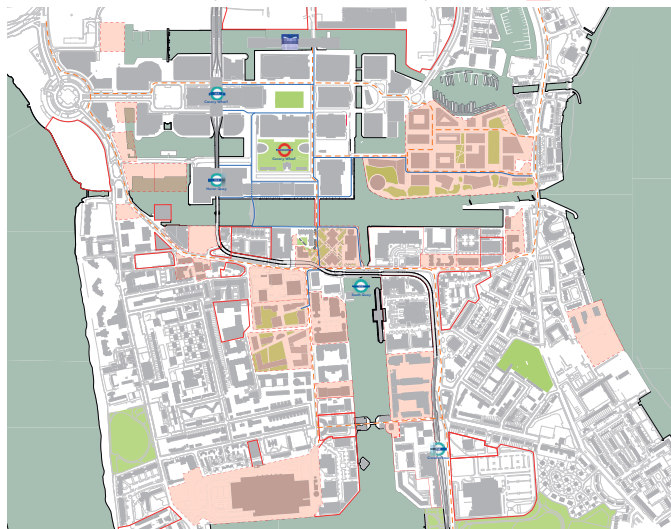
This exhibition shows proposed plans for the new bridge and illustrates how it might look. These are early concept designs, and more detailed designs will follow to support a detailed planning application later in 2018.

Your feedback will be reviewed and will inform the detailed design later in the year.



- DLR station
- Tube station
- Crossrail station
- Primary cycle routes
- Key pedestrian routes
- Permitted development sites
- Potential development sites

This plan shows how the new bridge will connect into the wider movement network, including to: the new Crossrail station to the north, South Quay Quay station to the south, Wood Wharf, and the wider cycle network



Pop-up consultation boards used at events

BOARD 2

SOUTH DOCK BRIDGE

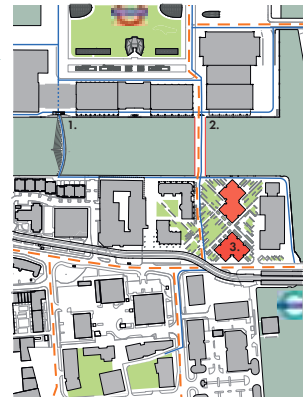
PUBLIC CONSULTATION

what is proposed?

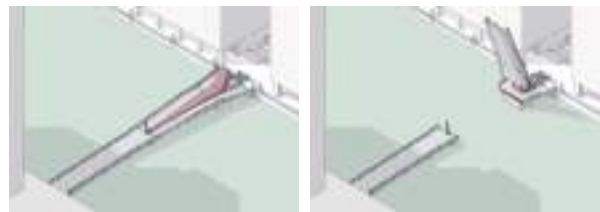
A new pedestrian and cycle bridge is proposed to connect Canary Wharf to the Isle of Dogs. This will align with Upper Bank Street on the north bank of the South Dock, and the Berkeley Homes 'South Quay Plaza' scheme on the south bank.

The bridge will have a lifting (bascule) mechanism to allow boats through when necessary and sufficient height to allow smaller boats through at all times. It is intended that the proposed bridge will be constructed from steel for lightness and strength.

1. Existing Wilkinson Eyre footbridge
2. Proposed crossing point of new Bridge
3. South Quay Plaza residential development and accompanying landscape. Currently under construction.



Plan of the bridge location and its links to the movement network



The above illustrations show a single leaf bascule bridge in open and closed positions allowing for a 25 metre navigation channel when open.

The bridge will integrate with the Berkeley Homes scheme being developed on the South Dock. Improvements are being considered to the landscaping on the northern Heron Quay, to improve access north to Upper Bank Street and the new Crossrail station in Canary Wharf.

The Team is working with key stakeholders to ensure an approach that balances the needs of all.

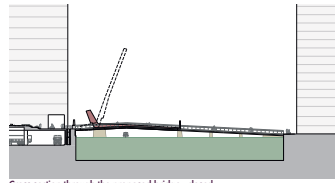


SOUTH DOCK BRIDGE PUBLIC CONSULTATION

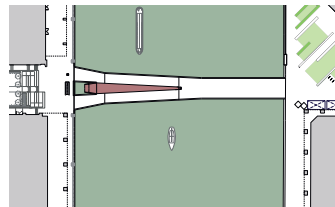
key considerations for the bridge design

The design must address a number of issues, which include:

1. The bank on the north dock is higher than the south dock, and the bridge must overcome this level difference, whilst having comfortable gradients for all users.
2. The approach areas must balance pedestrian flows with the role of these spaces for play and amenity - particularly on the south bank.
3. The bridge must be a minimum of 7.8m wide for pedestrians and cyclists to use comfortably.
4. The pedestrian and cycle paths will not be segregated as this would require a wider approach path than is available.
5. On the north bank, a basement exists with fire escape steps leading to dock level. This access must be retained.



Cross section through the proposed bridge - closed

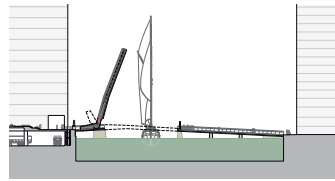


Plan of the proposed bridge - closed

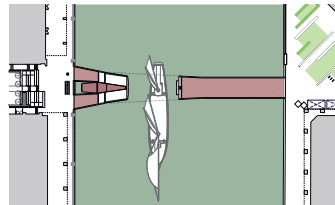
access for boats

The bridge must maintain access to the dock for boats, the key issues include:

1. The bridge must have a permanent navigable channel 15m wide for smaller boats to pass underneath.
2. The bridge must open to give a 25m wide navigable channel for taller boats.
3. Boat mooring points must remain available on the south dock wall.
4. The opening mechanism must be swift - the existing bridge requires long waits while it opens and closes.
5. Space should be available for pedestrians to wait on the bridge rather than having to wait on the dockside where there is limited space.



Cross section through the proposed bridge - open



Plan of the proposed bridge - open

Waiting areas on the bridge are shown hatched in red



SOUTH DOCK BRIDGE PUBLIC CONSULTATION

why do we need a new bridge?

Significant new development on the Isle of Dogs will generate greater pedestrian and cycling flows. The new Crossrail station will attract more pedestrians from the area and there is a need to improve access to South Quay DLR station. The existing bridge (also known as the Wilkinson Eye Bridge) is approaching its capacity at peak times in terms of comfort levels.

The new bridge will become one of the busiest pedestrian bridges in London.



3 Years

until the existing bridge will no longer be able to handle capacity



27,000

Pedestrians cross the existing bridge every day, the second busiest in London



70,000 - 80,000

Pedestrians are forecast to cross the South Dock everyday by 2030



2,500 - 4,500

Cyclists are forecast to cross the South Dock everyday by 2030



200 million

Passengers are estimated to use Crossrail every year

how will it help?

The bridge will integrate new development on the Isle of Dogs with Canary Wharf and Wood Wharf. It will share the load of new crossing demand with the existing bridge.

It will shorten walking and cycling times to the new Crossrail station and other public transport links, as well as improving access to jobs, retail and other town centre services at Canary Wharf. It will be fully wheelchair accessible and cater for all potential users.

The Council is also considering how the bridge will integrate with the wider walking and cycling network on the Isle of Dogs, which are due to be enhanced in coming years.

materials & finishes

The bridge will need to be strong, slender and light to allow boats to fit underneath and to support a lifting mechanism. It is, therefore, very likely to be made from steel.

However, the surface finish could vary and we would like to know your views on this. It could be left raw, to weather or be painted in a neutral, bright or dark tone. The surface of the bridge deck will be designed to be smooth and quiet to walk on.

STEEL LEFT RAW



A NEUTRAL TONE



A BRIGHT TONE



A DARK TONE



examples of opening bridges

Below are examples of bridges which open with a bascule mechanism. We'd like to know which of these you prefer.

Copenhagen, Denmark

Lightweight opening pedestrian & cycle bridge



Rotterdam, Netherlands

Counter-weighted opening pedestrian & cycle bridge



Whangarei, New Zealand

Rolling counter-weighted opening road bridge



BOARD 5

SOUTH DOCK BRIDGE

PUBLIC CONSULTATION

what is the process?



who is delivering the project?

The London Borough of Tower Hamlets is leading the delivery of this infrastructure and has assembled an expert project team to help deliver a high quality bridge and attractive public realm. The project is supported by **Transport for London**, who are providing policy and technical guidance.

The design team comprises: **Steer Davies Gleave** transport engineers; **Allies and Morrison** architects; and **Arcadis** engineers. Examples of the team's work are shown below.

So far, the project has been funded by the Council using contributions from development in the area, with some financial support from Transport for London. The Council is currently seeking additional investment in the project from other sources.

how to comment

We welcome your comments and suggestions; a six week consultation period is being held from **Monday 12th February 2018 to Friday 23rd March 2018**.
 Consultation responses can be made in a number of ways:

by post:
 Return your completed response form to:
Infrastructure Planning Team
Tower Hamlets Council
 Town Hall
 2nd floor, Mulberry Place
 5 Clove Crescent
 E14 2BG

online:
www.towerhamlets.gov.uk/southdockbridge

by email:
Infrastructure.Planning@towerhamlets.gov.uk



Projects by the design team

Previous work by Allies and Morrison
 Above: Ruckh's Road footbridge, Hackney 2012
 Below: Gornborough School footbridge, Hackney Wick 2014

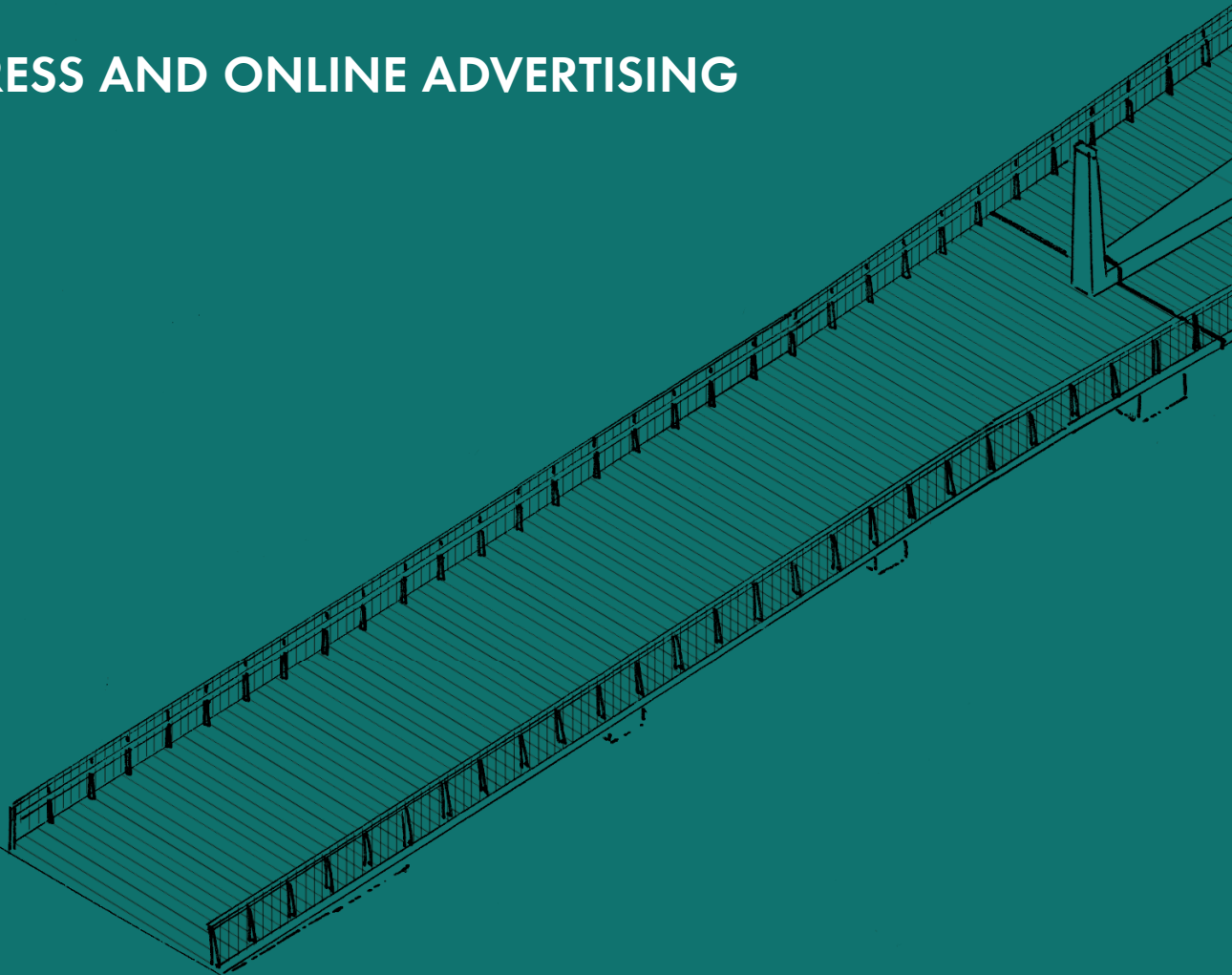
Previous work by Steer Davies Gleave
 Above: Strategic development of the Underground Northern line extension to Battersea, London (Ongoing)
 Below: Strategic development of the Montgomery Bridge at Wood Wharf, London (Ongoing)

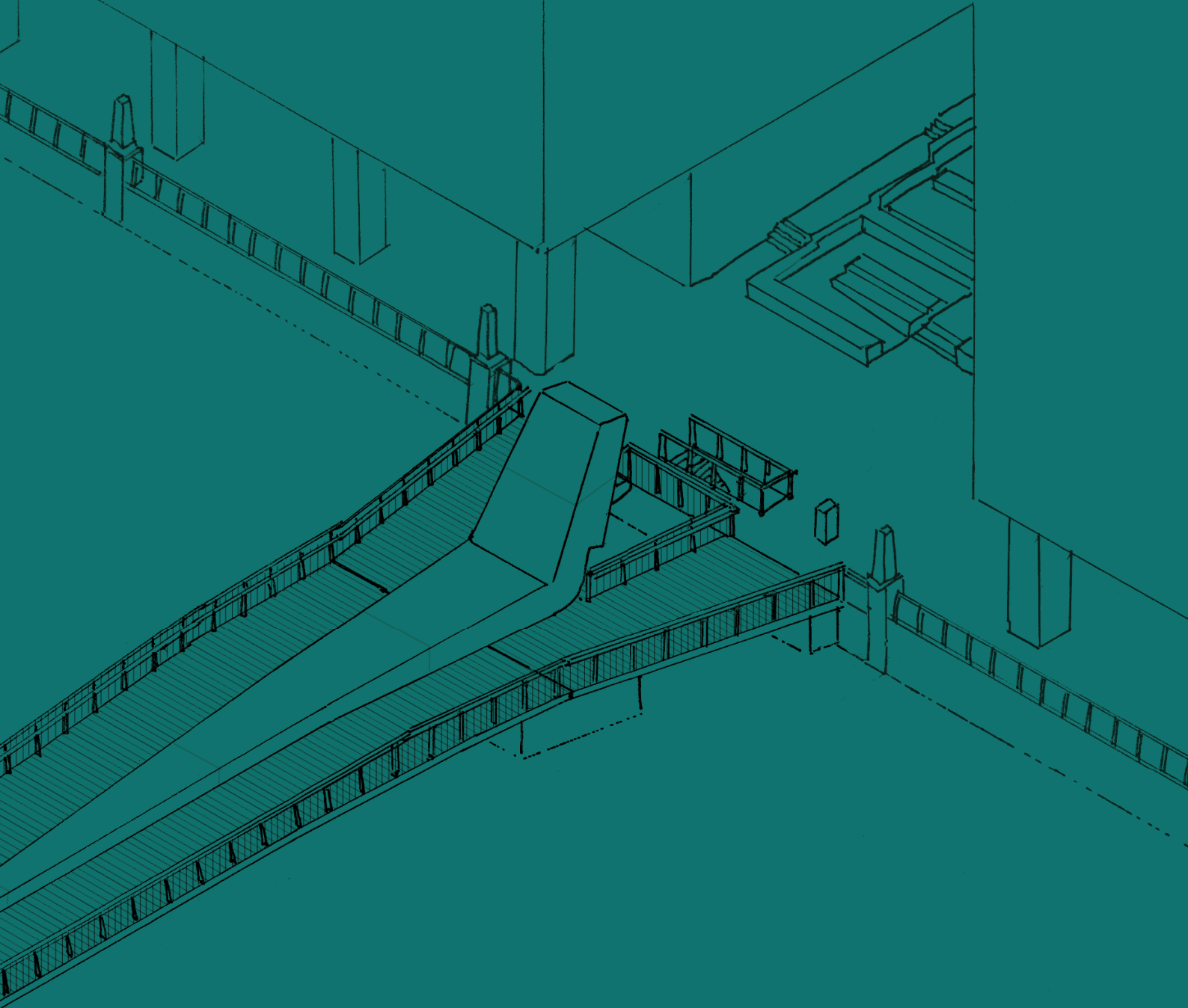
Previous work by Arcadis
 Above: Wynyard Crossing pedestrian and cycle bridge, Auckland Harbour, New Zealand 2011
 Below: Albert Bridge, Major strengthening and repair works, Chelsea, London 2011



APPENDIX C

PRESS AND ONLINE ADVERTISING






SOUTH DOCK BRIDGE

Consultation Report
May 2018

PRESS AND ONLINE ADVERTISING

To ensure that the most amount of people were made aware of the events, the council issued a press notice.


 Date: 7.2.18 Op: Bernie eSend: 354371
 Revise: 9.2.18 3: LES
 Size: 100x87mm DAM AH: Lisa Pub: Dochlands & East London Adv
PLEASE CHECK SIZE IS CORRECT

1260389TowerhamletsConsultx87 15:28 Fri, 09 Feb 2018

**LONDON BOROUGH OF TOWER HAMLETS
 PUBLICATION OF THE LONDON BOROUGH OF TOWER HAMLETS PUBLIC
 CONSULTATION ON THE SOUTH DOCK BRIDGE**

South Dock Bridge

The London Borough of Tower Hamlets is consulting on the new South Dock pedestrian and cycle bridge known as 'South Dock Bridge' and we invite you to submit your comments on the early concept bridge designs.

The public consultation will run from Monday 12th February 2018 until Friday 23rd March 2018

All feedback received will inform the detailed design later in the year.

The proposed bridge will connect South Quay and Canary Wharf and will help to support new development on the Isle of Dogs, and will help share the load of new crossing demand with the existing bridge. It will also shorten walking and cycling times to the new Crossrail station and other public transport links, as well as improving access to jobs, retail and services at Canary Wharf.

As part of this consultation the Council will be holding three public drop-in sessions. Locations and times are as follows.

Venue	Address	Date	Time
West Winter Garden	35 Bank Street, London E14 5NW	27th February 2018	11:00 - 15:00
Alpha Grove Community Centre	Isle of Dogs, London E14 8LH	3rd March 2018	10.00 - 13:00
Canary Wharf Idea Store	Churchill Place, London E14 5RB	8th March 2018	17:00 - 20:00

These events will provide the opportunity to view the public exhibition and find out more about the project.

Further information and details of how to respond to the consultation can be found on the Council's website: www.towerhamlets.gov.uk/southdockbridge, by emailing Infrastructure.Planning@towerhamlets.gov.uk or in the Council's Idea Stores and the Town Hall.

For further information, please contact the Infrastructure Planning team on 020 7364 5241.
 Dated: 12th February 2018.

Press notice issued by LBTH

Secure | https://www.towerhamlets.gov.uk/news_events/news/2016/February_2016/South_Dock_Bridge_consultation_starts

Online Services | Contact us | News & Events

Apply for it | Pay it | Report it | Request it | Sign up

Navigate the site | Enter keywords | Search

Home | News & Events | News | South Dock Bridge consultation starts


More in this section

News

2016

- 2016-17 budget agreed by Full Council
- All set for The Big Half
- Ask the Mayor - 15 February 2016
- Day number 1 consultation - council response
- Electoral concerns? Report it!
- Leave your rat on the table for Tower Hamlets' future
- Improved Youth Service is launched in Tower Hamlets
- It's Time to Talk as council launches suicide prevention strategy
- Make sure you're prepared for the snow
- Marina wants to be a new boiler
- New enhanced service for drug and alcohol users launched
- New powers to combat anti-social behaviour in social housing
- Operation Continuum strikes again
- South Dock Bridge consultation starts
- Temporary closure of Bethnal Green Library
- Tower Hamlets sets the standard for affordable homes

South Dock Bridge consultation starts



Plans for a pedestrian and cycling bridge that will link South Quay and Canary Wharf in the face of Dockside congestion on the existing South Quay Footbridge (also known as the Millennium Eye Bridge) and significantly reduce journey times across a busy area of Tower Hamlets are being consulted on from this week.

The proposed 'South Dock Bridge' agreed by Tower Hamlets Cabinet will support the large amount of development coming forward on South Quay and the Isle of Dogs and will relieve congestion on the existing South Quay Footbridge and the DLR.

The council agreed an expenditure of £230,000 for preparatory work, including work on the initial design and consultation to progress the South Dock Bridge Project.

To share your views please visit the [South Dock Bridge consultation](#) page where you can submit your comments up until 23 March.

Three consultation events are planned at which you can see plans and speak to officers who are involved in the projects as follows:

Tuesday 27 February 2016, 11am-3pm
 West River Garden
 35 Bank Street
 London
 E14 5NW

Saturday 3 March 2016, 10am-1pm
 Alpha Grove Community Centre
 916 of Dogs
 London
 E14 5JH

Thursday 8 March 2016, 5pm-8pm
 Canary Wharf Idea Store
 Churchill Place
 London
 E14 5BB

The council's proposed bridge location aligns with Upper Bank Street and has been selected following the assessment of different locations. The chosen alignment will enable new and existing residents, and other users to better access transport links such as the Jubilee Line and the new Elizabeth Line stations, retail shopping and town centre services in Canary Wharf.

The council has committed Community Infrastructure Levy (CIL) funding to the next phases of development and intends to make a further contribution towards the delivery of the bridge at a later stage.

Feedback from the consultation will help inform work on the detailed design of the bridge, with a planning application proposed to be submitted later in the year.

Mayor of Tower Hamlets John Biggs said:

"I have called for this bridge to be built long before I was Mayor, so am delighted that I've managed to finally get things moving. The Isle of Dogs requires infrastructure to match the development in the area, relieve congestion on the existing bridge and improve cycling and pedestrian access. It's committed to delivering this for residents."

Councillor Arina Ali, Cabinet member for Development, said:

"As a council we have set out to reduce the impacts of traffic on our residents, making our borough one of the best in London to walk or cycle in. This bridge delivers on that commitment for the area - a very much needed and I expect very well used resource. It serves our aspiration to have a cleaner greener fair borough and I look forward to hearing residents' views."

Residents of the borough will have the chance to comment on the design of the bridge via an on-line consultation as well as having the opportunity to give comments by post. The consultation will start on 12 February 2016 and will run until 23 March 2016.

Tower Hamlets is the fastest growing borough in the country with a current population of 160,000 that is expected to grow to 205,000 by 2031.

The area that the bridge will serve is one of the densest areas in Tower Hamlets. Providing sufficient housing and infrastructure to meet the current and future demand is a strategic priority for London's local authorities, the Mayor for London and the government.

The borough's emerging Local Plan identifies that the Isle of Dogs and South Poplar will need to accommodate 20,000 new homes and 78,875 new jobs by 2031.

A feasibility study undertaken over the last three years estimates that the bridge will have the London's second highest pedestrian use - second only to the Millennium Bridge in central London.

Posted on Monday 12th February 2016

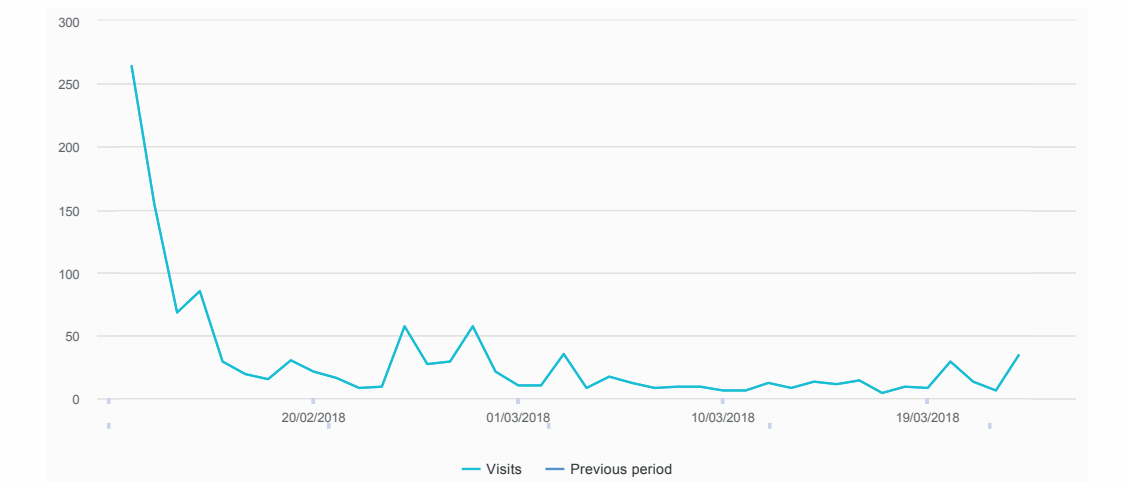
Screenshot of press release issued by LBTH

Page
South Dock Bridge consultation
htt
p://
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Period
12/02/2018 - 23/03/2018

Compare to Align weekdays

Previous period Yes

Visits 1,200 +1,200	Page views 1,735 +1,735	Unique visitors 1,033 +1,033	Returning visitors 214 +214	Bounce rate 49.75% +49.75
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TOWER HAMLETS