

King Edward Memorial Park Permanent Works Application

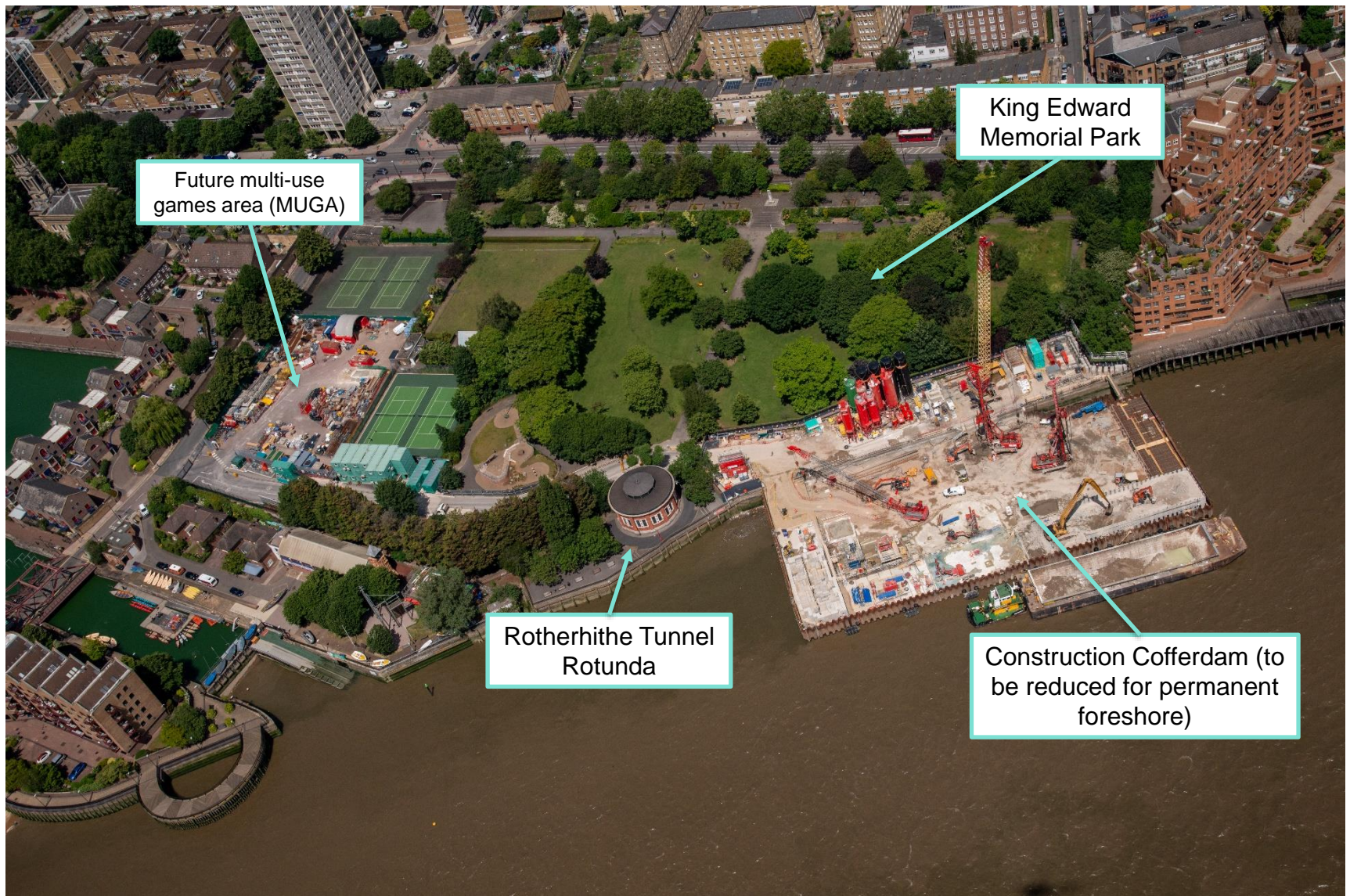


Tideway

Introduction to Proposals

**The Site and the Development Consent Order Constraints
and Opportunities**

The Current Site (photo taken Autumn 2019)



The Permanent Works Proposals

The details submitted as part of this application are to discharge specific Requirements (similar to planning conditions) set out on the Development Consent Order (DCO) that was made in September 2014. The DCO authorised the Thames Tideway Tunnel Project, including the principle and general location of the foreshore and permanent above ground structures on it, subject to approval of these further details. The Requirements in question relate to the permanent works at the application, and are as follows:-

- KEMPF2 – Location of permanent works
- KEMPF3 – Detailed design approval for permanent above-ground structures
- KEMPF4 – Detailed design approval for signature ventilation columns
- KEMPF5 – Detailed design approval for river wall and foreshore structure
- KEMPF6 – Landscaping works
- KEMPF 14 – Surface water drainage
- PW11 – Interpretation strategy (how proposed art relates to site history)

In addition, the submitted scheme has been prepared in line with the approved Designed Principles document as part of the DCO. Further details are located at Section 4.2 of the Supporting Statement.

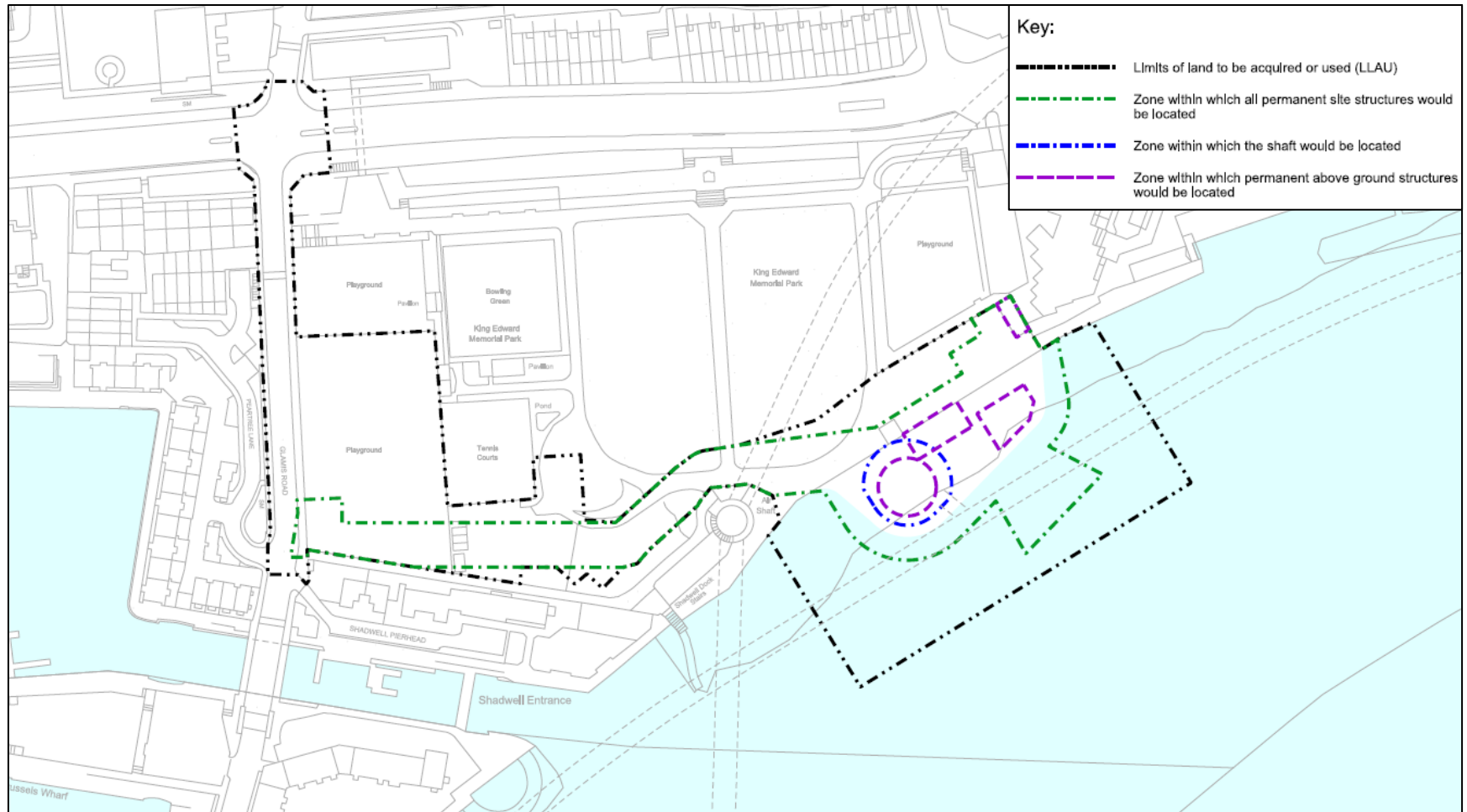
The Permanent Works Proposals

The proposed development subject to approval comprises the following elements:-

- a. Hard and soft landscaping to form the foreshore development;
- b. 2 no. vortex columns and 3 no. ventilation columns;
- c. A new electric and control kiosk (including a brown roof);
- d. Minor structures including the relocated bandstand, street furniture, the park gate and fencing, and balustrades and handrails;
- e. Detailed design for the river wall and foreshore structure;
- f. Details of surface water drainage; and
- g. Details of the artwork commissioned as part of the Tideway's Art and Heritage Interpretation Strategy.

DCO Parameters (Constraints and Opportunities)

The below lines set out the parameters approved in September 2014 that the proposed permanent works must comply with. Permanent structures cannot deviate outside of these parameters.



Proposed Foreshore Design



The Proposed Scheme

Context for the Design

The below demonstrates the relationship between permanent works scheme that is subject to this application and the park, including the land subject to the wider masterplan being brought forward separately by the Council.



Integration with King Edward Memorial Park Masterplan

The below demonstrates the relationship between the proposed works and the proposed masterplan.



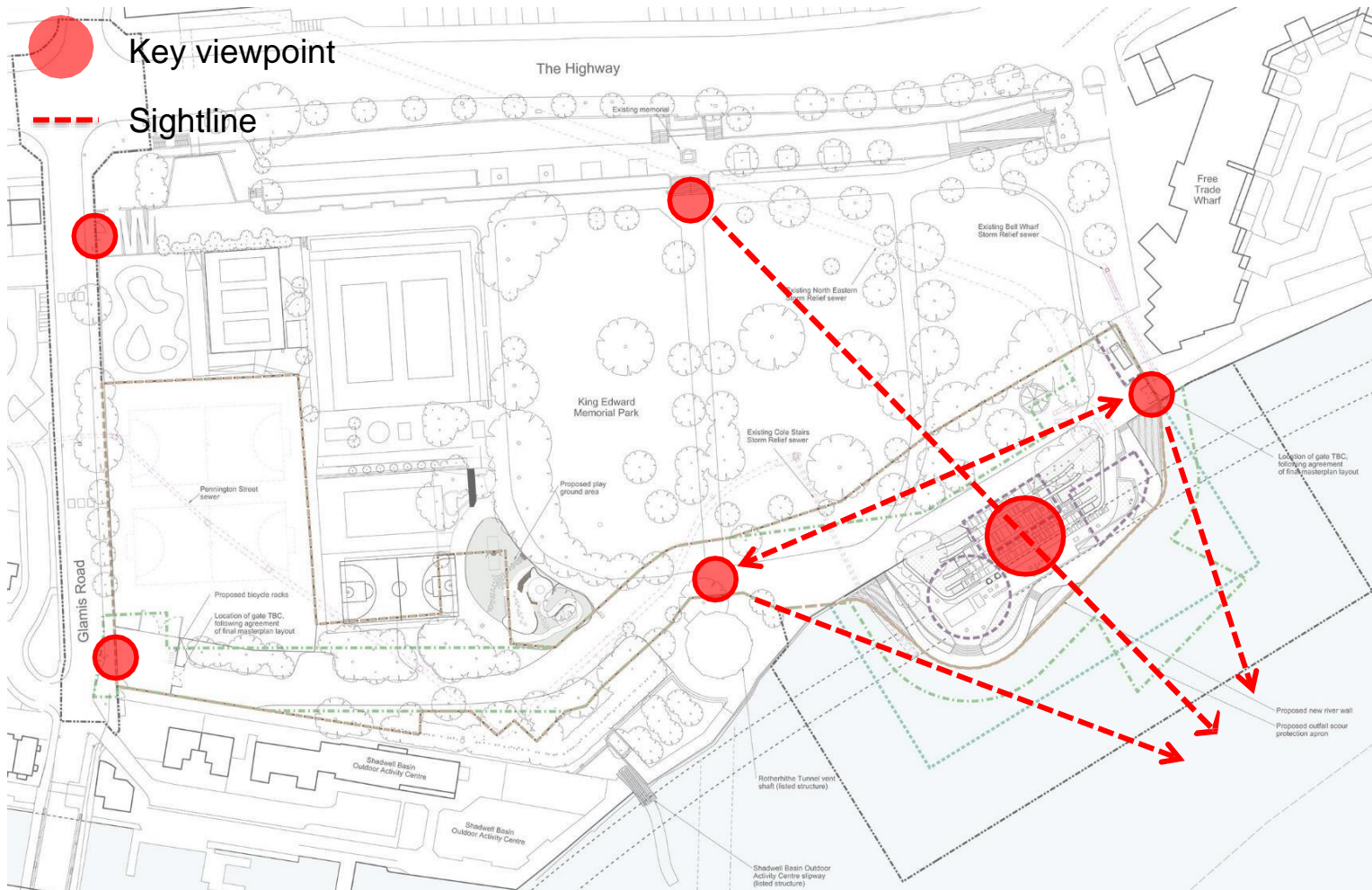
Key Considerations and Design Objectives

This graphic sets out the starting point for the development of the permanent works scheme. It identifies the existing pedestrian routes that the scheme will protect and enhance through sensitive integration with the overall public realm as well as the area of works required for development.



Key Park Views

The below demonstrates the key park views that have been maintained in the design of the foreshore. The scheme has sought to maintain and enhance views from park entrances and path junctions to the river and across the foreshore where possible.



Key Park Views

The below image shows King Edwards Memorial Park before the Tideway project. There is opportunity to open up views across the park.



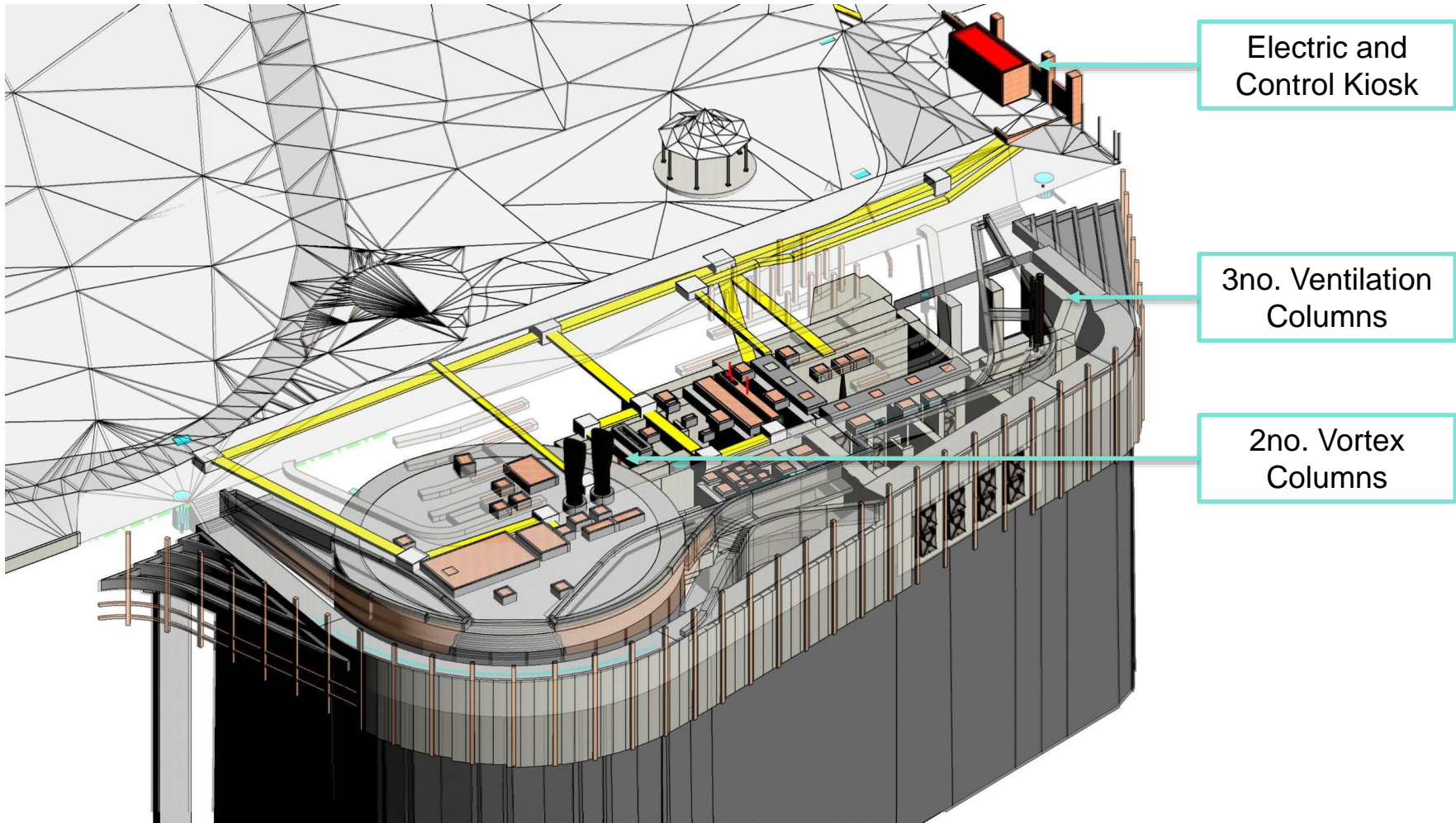
Key Thames Views

The below shows the key views that have been considered in the design of the foreshore.



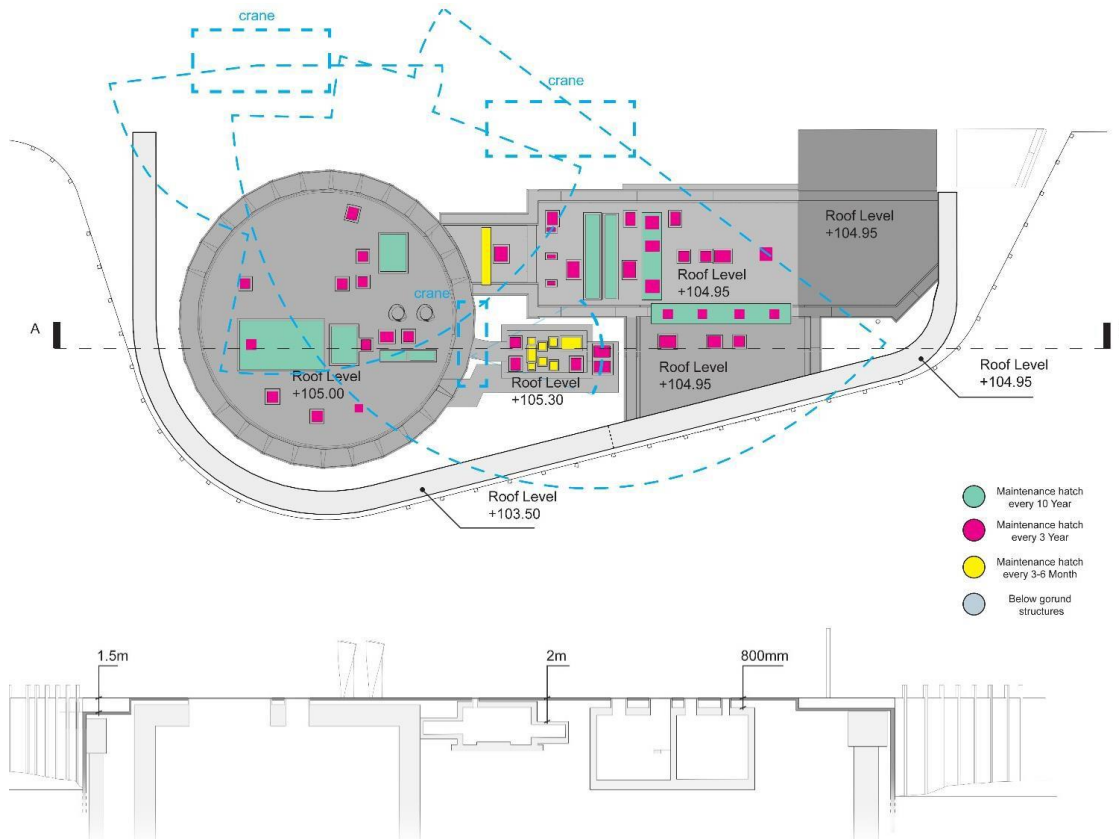
Infrastructure Constraints

This infrastructure graphic sets out the below-ground infrastructure that has been installed as part of the project. The siting of these structures are constraints for the locations of the permanent above-ground structures required (see purple dotted line on the parameter plan).



Infrastructure Constraints

This infrastructure graphic sets out the maintenance requirements at the site, and the subsequent requirements for hardstanding on the foreshore to enable access to the below-ground infrastructure.



- Replacement of large equipment every 60 years (e.g. penstocks)
- Inspection /maintenance of tunnel every 10 years
- Replacement of equipment every 15 years (e.g. dampers in shaft)
- Replacement of air treatment media every 3 years
- **Yearly / 6 monthly** inspections and maintenance (e.g. below ground instruments and flap valves)

The Proposed Development

Consultation and Design Evolution

Community and LBTH Consultation

Authority	Date	Purpose
LBTH	25.01.17	Discussion to set up future engagement.
	23.02.17	Design presentation, 'floodable' public realm, scale of central space, accessibility, vent columns, views, materials.
	30.05.17 and 29.11.17	Masterplan interface discussions.
	25.07.17 and 07.09.17	Presentations and discussions relating to ventilation columns, kiosk, planting and furniture locations.
	25.05.18	Also attended by Historic England & Marine Management Organisation (MMO) – general design, ventilation columns, planting, materials, accessibility, furniture.
	Regular monthly meetings to present day	Monthly meeting to discuss design progress and matters arising.
Public consultation events	04.06.16	TTT participation in consultation event in park organised under LBTH Masterplan, to gather community priorities and ideas for foreshore and Tideway site design.
	Sept 2016	TTT attendance at masterplan follow up consultation exhibition.
	24.03.17	Public drop-in event in Shadwell Centre – for both Tideway scheme and LBTH Masterplan; including Tideway permanent and temporary [hoarding] art, with artists present.
	22.05.17	Public event in Shadwell Centre. Combined LBTH masterplan with Tideway designs and art. TTT presentation included.
	12.12.17	Additional community event with TTT artist Hew Locke.
	14.03.18	Community Liaison Working Group [CLWG] in Shadwell Centre. TTT presentation and update, mainly on ventilation columns.
	Date unconfirmed	Discussion with CLWG Architects on safety arrangements relating to overhangs at intertidal terraces.

Community and Stakeholder Consultation

The below sets out issues that have been raised through stakeholder and community consultation as the design has developed, and how they have been addressed in the submitted scheme.

Issue Raised	How Addressed
Integration <ul style="list-style-type: none"> The design of the foreshore should work as a cohesive whole, not a sequence of individual components The site should create visual linkages between the new foreshore and the older park area, particularly from the highway It is a key strategic aim that materials, sight lines and art flows between the foreshore and the wider masterplan. Tideway should explore the possibility of combining of Kiosk element of the foreshore with the masterplan Café 	<ul style="list-style-type: none"> Proposed trees have been moved out of the sightline from the north of the park near the King Edward Memorial to the river. The central space on the new foreshore has been kept clear to maximise the river view. The same materials have been specified for the proposed paths as proposed by the masterplan for resurfacing the park paths. Selected materials have been informed by the existing fabric of the park. The art plinths are located on the new foreshore and to create a trail from the Glamis Road entrance. The kiosk location and size are determined by the Development Consent Order, our planning permission.
Accessibility <ul style="list-style-type: none"> The foreshore should be fully accessible to all the community – taking special care to ensure access to older people, people with limited mobility and people with young children 	<ul style="list-style-type: none"> The design of the lower path has been amended to meet Building Regulations requirements. All areas of the new foreshore structure are now accessible to people with pushchairs and to wheelchair users. The design now incorporates a wide range of seating, from informal concrete seating walls to timber-topped benches with backs and armrests; easy for older people to use.
History and Heritage of King Edward Memorial Park <ul style="list-style-type: none"> LBTH has noted that the park has a proud history which is significant to local residents and of interest to visitors. The masterplan and foreshore should value this history, telling the story of the park and of those who lived and worked nearby. 	<ul style="list-style-type: none"> At a public event in December 2017, residents provided the scheme artist, Hew Locke, with a detailed list of historical information about KEMP and its neighbourhood. The artist has designed 6 bronze ships that will incorporate details relating to the site history. Proposed signage will include interpretation of the artwork and the heritage themes, as well as an explanation of the project and general wayfinding.

Community and Stakeholder Consultation

Issue Raised	How Addressed
<p>Quality</p> <ul style="list-style-type: none"> It is important that the special qualities of the site heritage and views of the river are set in an exciting and dynamic context. The park and foreshore should include design and materials of the highest quality. Creative use of lighting should be used as a feature and to highlight features. 	<ul style="list-style-type: none"> The scheme for KEMP is completely site-specific and like no other landscape in London or elsewhere. The public realm architects challenged the previously indicative scheme, which was a single-level, large flat surface. The structural river wall now steps down to allow the formation of the lowered paths around the river edge. This modelling of the ground plane will offer a new and unique experience of the river. Materials of the highest quality have been selected. One of the DCO requirements states that there will be no lighting [other than for emergency maintenance inside the kiosk].
<p>Soft landscaping</p> <ul style="list-style-type: none"> Soft landscaping should be maximised and hard landscaping is minimised. It is acknowledged that out of necessity the foreshore will contain a lot of hard landscaping so as much soft landscaping as possible should be incorporated into the design. More shade by trees have been requested. 	<ul style="list-style-type: none"> The scheme has sought to soften the design by including a curved river wall and flowing, curving lines to paths and beds, achieved by using a flexible metal edging to path surfaces and curving wooden and concrete benches on the lower path. The soft landscape has been raised at the sides of the foreshore structure as mounds rather than flat beds. 3 intertidal terraces have been planted with marginal aquatic vegetation to increase greenery and biodiversity. 6 small trees have been included beside the seating in the central space, to give shade and extra greenery to the space.
<p>Interactivity</p> <p>The park and foreshore should engage park users, be interactive and not static. It should include:-</p> <ul style="list-style-type: none"> Features that engage park users Art you can climb and sit on rather than just visually enjoy Movement and dynamism in surfacing and installation 	<ul style="list-style-type: none"> The ground levels have been modelled specifically to address concerns. The outer lower path, intertidal terraces, green mounds and central space with integrated features break down the visual mass of the structure and add visual interest. Many bespoke landscape elements have been included which park users can engage with physically. These pieces will be tactile and rich in historic and imaginative detail but are not designed primarily for climbing. The surfaces have been designed to encourage different kinds and speeds of movement, including cycling and running, as well as leisurely strolling.

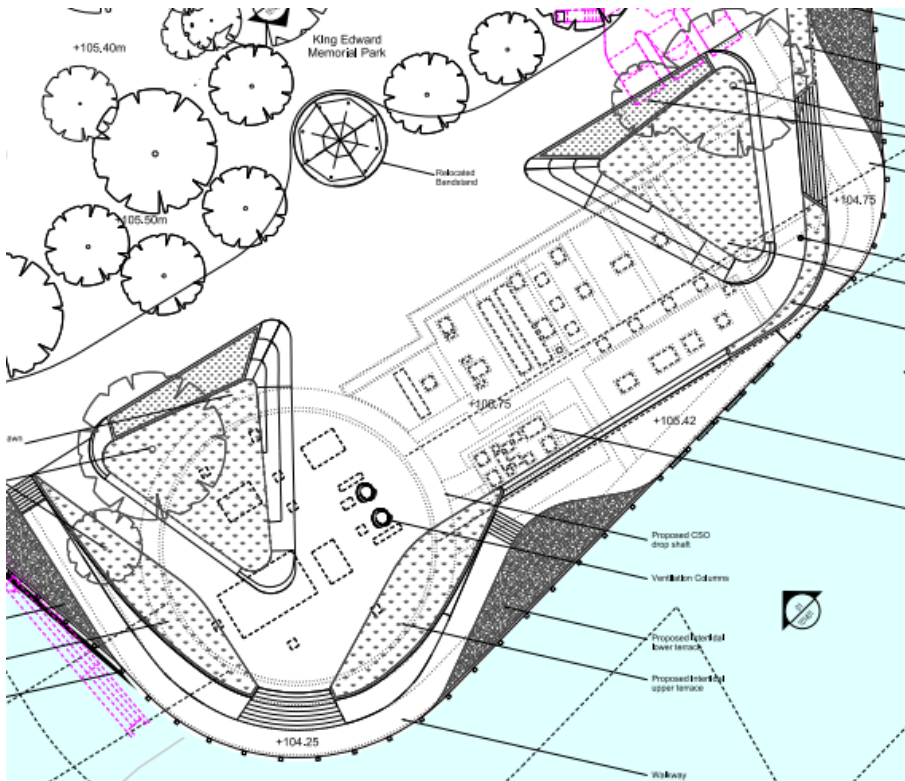
Community and Stakeholder Consultation

Issue Raised	How Addressed
<p>Views of the river</p> <ul style="list-style-type: none"> • The views of the river are the central feature of the park, and the new foreshore must enhance rather than impede views of the river. The water, access to it and views of it is essential for the history of the site and sets the proportion and scale of the site. • The scheme should avoid obscuring views and use water features to emphasise its river location – inundation (tidal flow onto terraces), water collection points, fountains for park users to interact with. 	<ul style="list-style-type: none"> • As a result of this comment proposed trees have been moved out of the sightline from the north of the park near the Memorial [an important path junction] to the river. With a continuous line of trees along the river edge, the previous tree line did impede the river views that are the central. • The central space on the new foreshore has been kept clear to maximise the river view, framed by the two mounds at either side. • The mounds have been designed with grass slopes and sitting edges facing east and west where people can enjoy the long views downriver to Canary Wharf and upriver. • Balustrading has been excluded wherever possible. Previously, the railings along the river cut across the eye level of visitors sitting on benches. Instead, beds of low planting have been used to protect against falling without obstructing vistas. • Where balustrades are essential for safety reasons – i.e. around the edge of the lower walkway – they have been specified as transparent stainless steel wires to disrupt views as little as possible. • A lower walkway around the edge of the new foreshore structure has been incorporated to offer a fresh experience of the Thames. • Intertidal terraces have been introduced that will inundate, allowing closer observation of the tidal processes and of special waterside vegetation [kept low at main viewing points to maintain clear sightlines to the river]. • The surface water drainage is designed so that the majority runs northward to collect in sustainable drainage ‘filter strip’ areas below the grass.

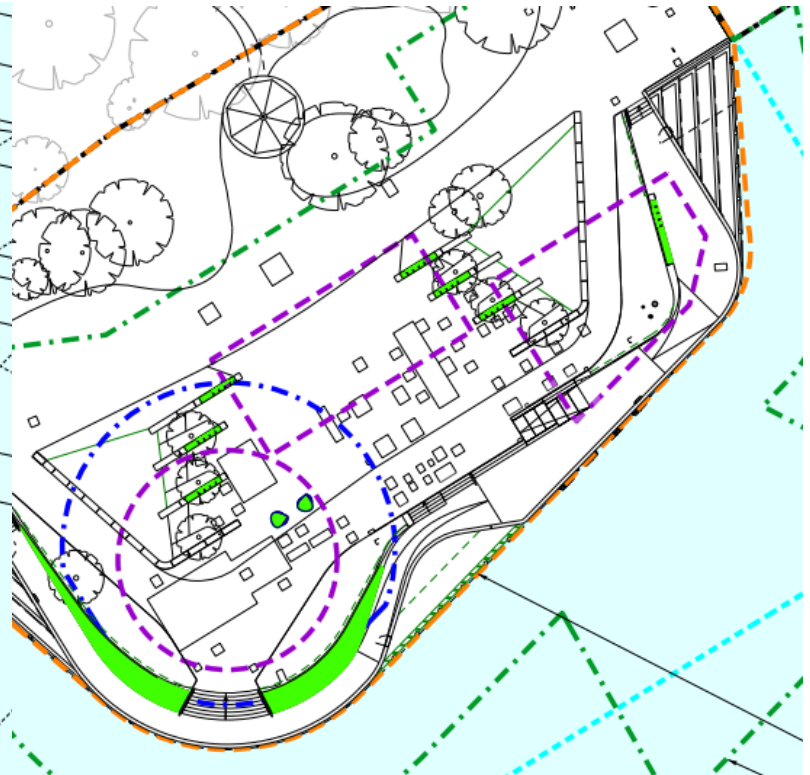
Opening up views of the river

The bandstand has been moved to the east to open up views of the river from the north of the park. There is also an opportunity to reconsider proposed tree locations to further open up this view.

Centrally located bandstand



Bandstand moved to the east



Mass and scale

Additional planting and seats that extend out into the space have been incorporated into the central 'square' to break it up. The sculpted ground levels break up the mass of the foreshore structure and permit a variety of different experiences.

DCO design option



Current proposal



Accessibility

A sloping path has replaced steps between the different levels on the lower path to improve accessibility.

Stepped access to lower path



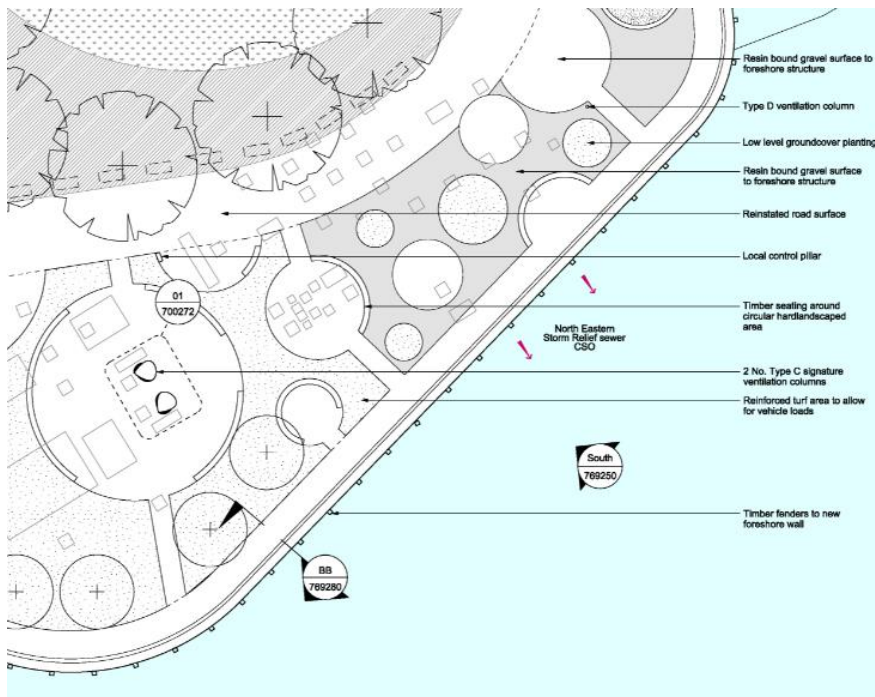
Sloping access to lower path



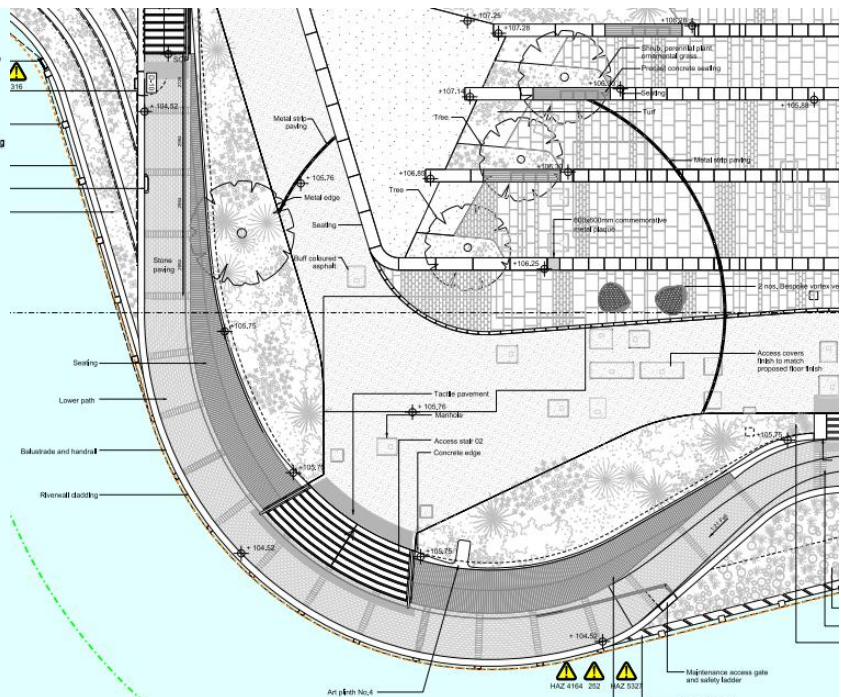
Materials that reflect the park fabric

The proposed material palette has been developed to better reflect the park fabric. The lower path surfacing has changed from resin bound to stone paving and stone paving has replace concrete in parts of the square.

Resin bound foreshore surfacing



Buff coloured asphalt foreshore with stone paving lower path



Integration with the park

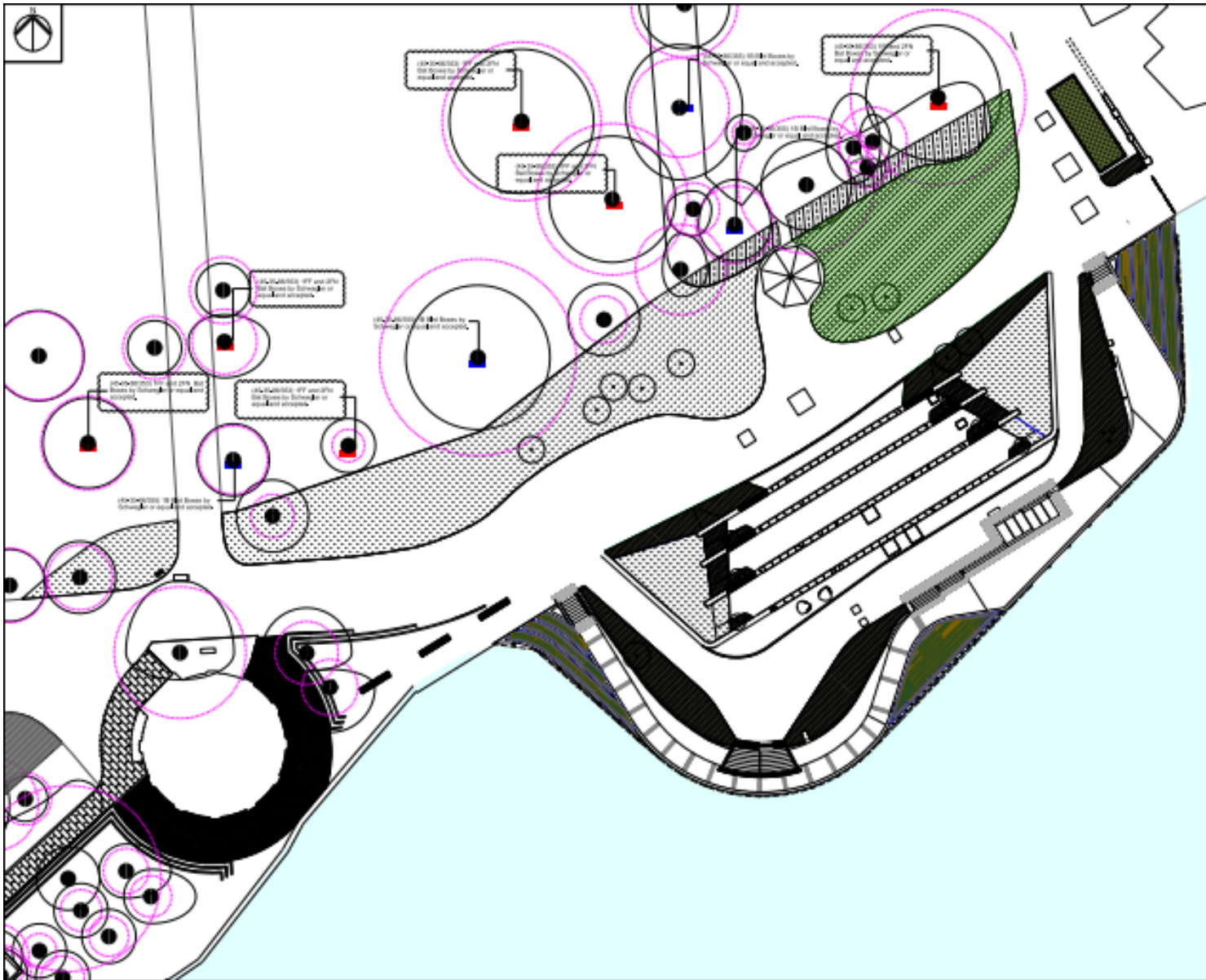
The scheme integrates with the King Edward Memorial Park and LBTH masterplan, creating a wider and improved Thames Path to accommodate cyclists and runners.



The Proposals

Design of the Proposed Scheme

Detailed Design of Foreshore Site



Detailed Design of Glamis Road Approach



View of the Foreshore at High Tide



View of the Foreshore at Low Tide



View to South East from Central Square

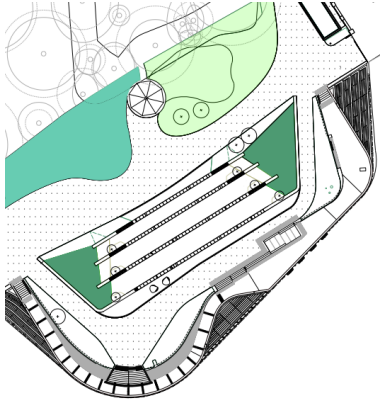
Visualisation showing the view of the River Thames to the south east from the central square



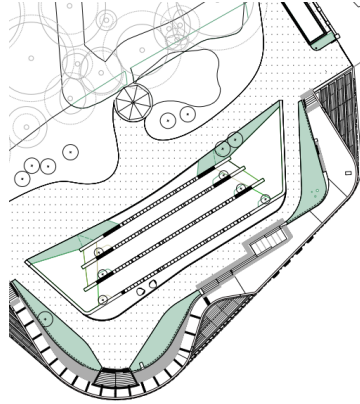
The Proposals

Landscaping Works

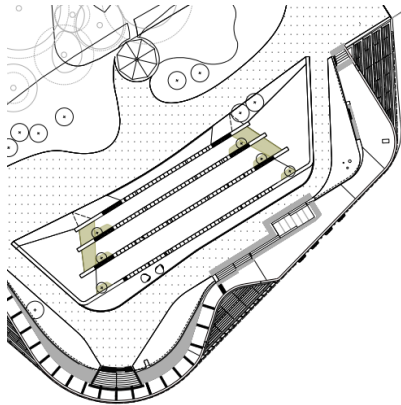
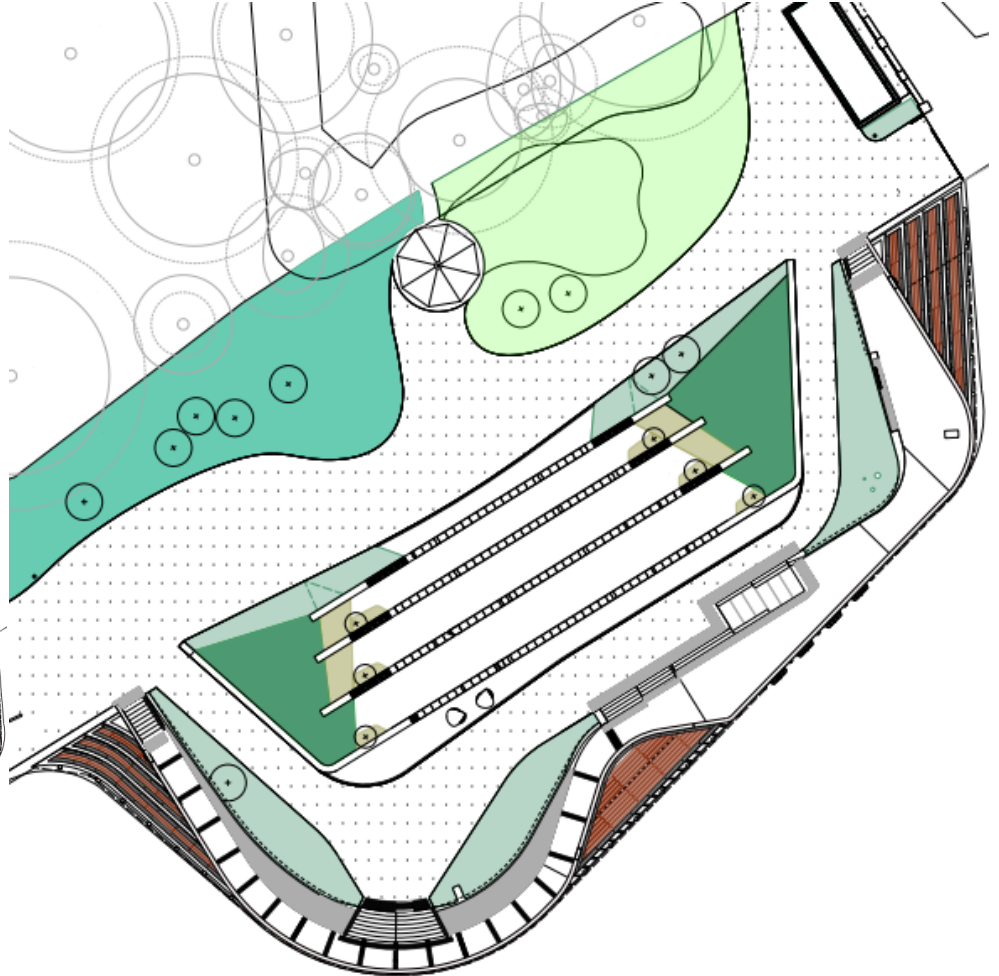
Greening of the Foreshore – Foreshore Planting



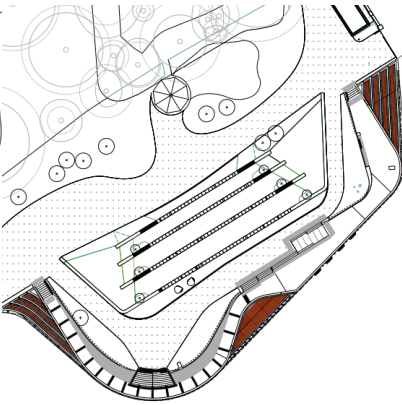
Lawn turn planting



Shrub and perennial planting



Ground cover planting



Inter-tidal planting

Greening of the Foreshore – Foreshore Planting

Shrub planting



Tree canopy with shrubs



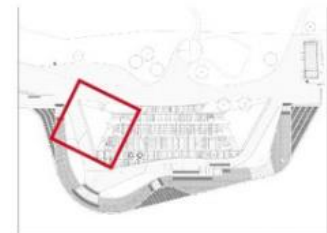
Footpath

Shrubs, ferns and grasses

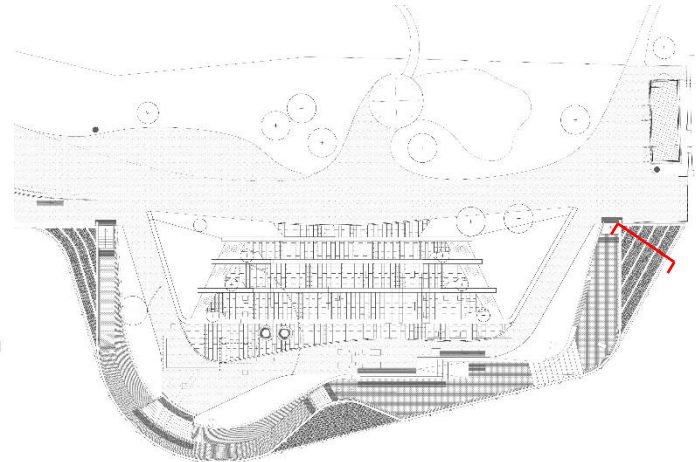
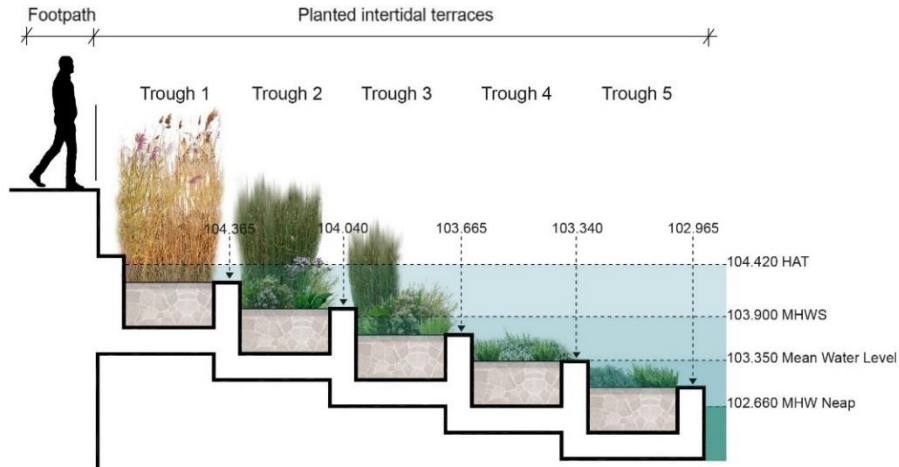
Footpath



Greening of the Foreshore – Foreshore Planting



Intertidal Terrace Planting



The planting palette will include the following species:

Trough 1 (upper most)
100% *Phragmites australis*



Phragmites australis

Trough 2
25% *Schoenoplectus maritimus* (Sea club rush)
20% *Limnium vulgare* (Sea lavender)
10% *Elymus pycnanthus* (Sea couch)
10% *Elymus repens* (Couch grass)
10% *Puccinellia maritima* (Common saltmarsh grass)
10% *Aster tripolium* (Sea aster)
10% *Beta vulgaris* subsp. *Maritima* (Sea Beet)
5% *Plantago maritima* (Sea Plantain)



Schoenoplectus maritimus



Limnium vulgare



Elymus pycnanthus



Elymus repens

Trough 3
30% *Juncus maritimus* (Sea rush)
10% *Puccinellia maritima* (Common saltmarsh grass)
10% *Elymus pycnanthus* (Sea couch)
10% *Elymus repens* (Couch grass)
10% *Aster tripolium* (Sea aster)
10% *Triglochin maritima* (Sea arrow-grass)



Puccinellia maritima



Aster tripolium



Beta vulgaris subsp. *Maritima*



Plantago maritima



Juncus maritimus



Glaux maritima

Trough 4
50% *Halimione portulacoides* (Sea purslane)
20% *Puccinellia maritima* (Common saltmarsh grass)
10% *Spartina angelica* (Common cord grass)
10% *Salicornia europaea* (Marsh samphire)
10% *Glaux maritima* (Sea mallow)



Triglochin maritima



Halimione portulacoides



Spartina angelica



Salicornia europaea



Spartina maritima



Suaeda maritima

Trough 5 (lowest)
60% *Spartina angelica* (Common cord grass)
20% *Salicornia europaea* (Marsh samphire)
10% *Spartina maritima* (Common salt marsh grass)
10% *Suaeda maritima* (Sea blite)

Species shown indicatively. Please refer to Planting Schedule

Materials

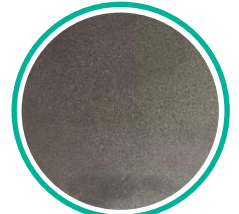
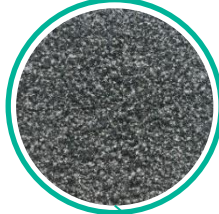
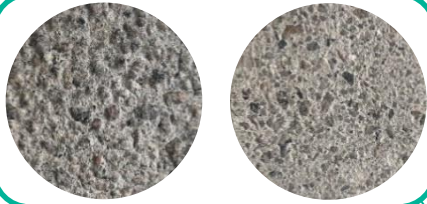
Concrete

Granite

Cast iron
Untreated / Painted Black

Hard wood

Mild steel



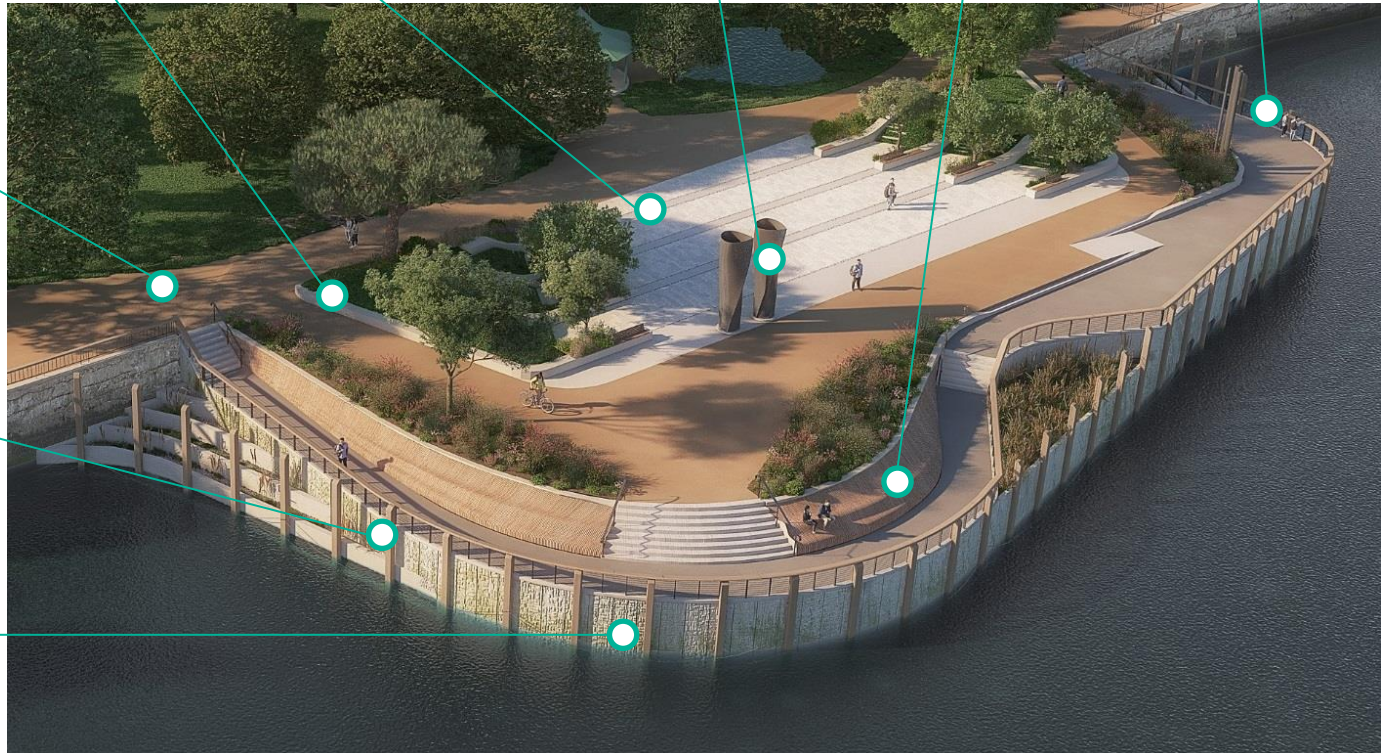
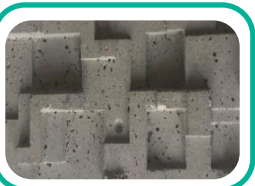
Asphalt
Buff coloured



Hard wood



Ecological Concrete



The Proposals

Main and Minor Structures

2no. Vortex Columns

Visualisation of the proposed vortex columns on the foreshore with inset of location, as stipulated within the approved DCO parameter plans. Passive filters are located below ground to clean the air and remove odours before being emitted from the columns.



Pattern Development of Vortex Columns

This shows the basis for the pattern development on the vortex columns.



Pattern of the foreshore meeting the river with strong simple line work inspired by the Rotunda grilles



3no. Ventilation Columns

Visualisation of the proposed ventilation columns in the eastern foreshore planting bed with inset of location, as stipulated within the approved DCO parameter plans. Passive filters are located below ground to clean the air and remove odours before being emitted.

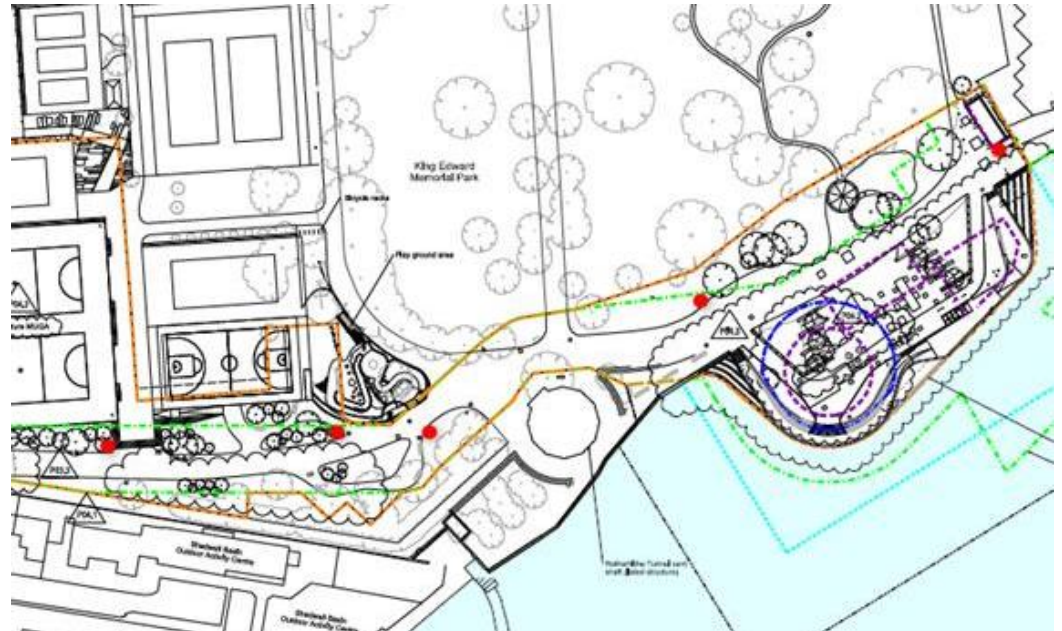


Electric and Control Kiosk

Visualisation of the proposed electric and control kiosk with inset of location, as stipulated within the approved DCO parameter plans. The Kiosk is located to minimise visibility and avoid interrupting views to the river. Brick cladding is proposed for the back of the kiosk that will match the existing park boundary wall and complement the Free Trade Wharf boundaries. The kiosk will also have full height vertical hardwood fins to the front, northern and southern sides.



Litter Bins



No. 4 Heritage Cast Iron Litter

Bin BX 2361

Standalone Benches

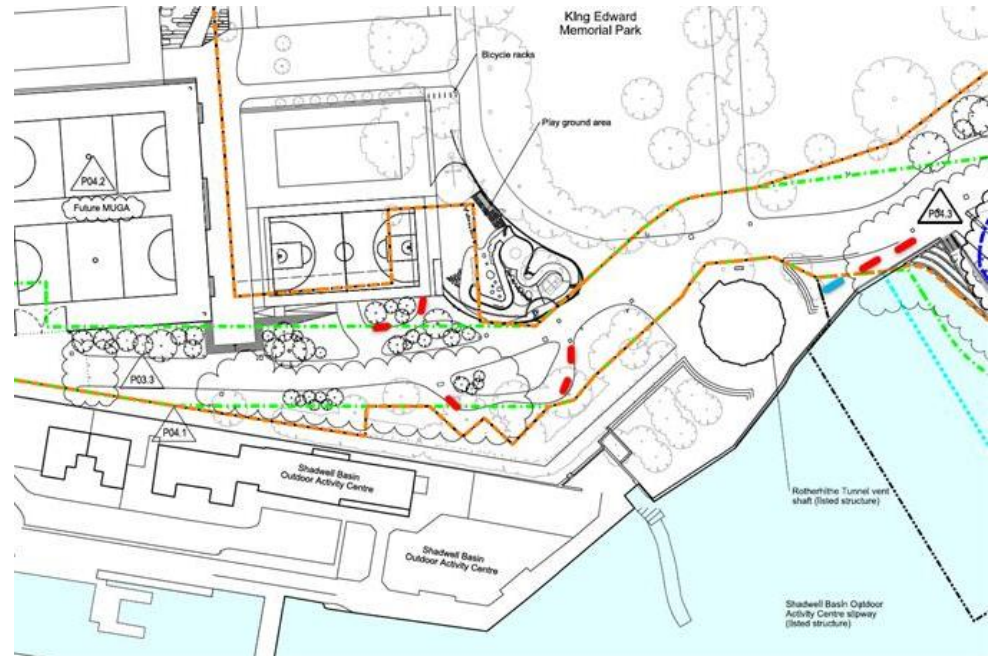


No.9 New Forest Seat

No.1 New Forest Seat
proposed to be placed outside
the DCO

Frame: Cast iron, painted finish

Slats: Cumaru/Iroko timber, smooth planed finish



Foreshore Benches

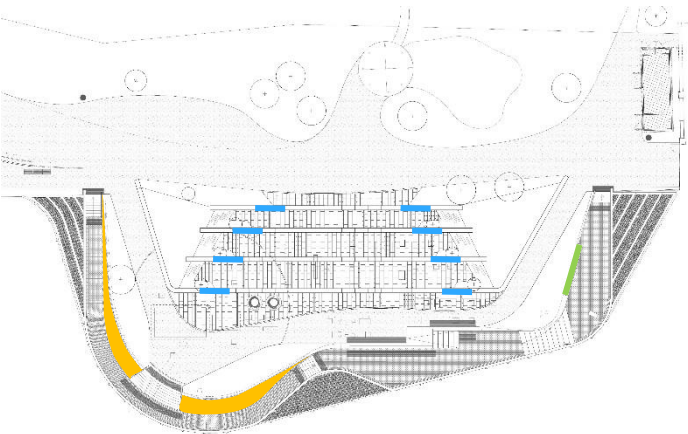
Central space bench



Central lower walkway bench



Eastern lower walkway bench



Art Strategy

Art Strategy

Artist & Proposal

- 6no. precast concrete plinths are proposed at the site.
- Interpretive artwork material has been developed by an artist, Hew Locke, appointed by Tideway, in collaboration with the Design Team, according to Tideway's Art and Heritage Interpretation Strategy.
- The artist proposes six unique cast bronze boats representative of the history of the Thames and of the shifting population of the local neighbourhood, placed on precast concrete plinths around the Tideway site.
- These currently include: a Tug, Chinese Junk, Thames 19th Century Paddle Cruiser, John Hawkins (slave ship), Thames Barge and a Bangladeshi river boat.
- The meter-long boats will encourage touching, curiosity and interaction, and set people off on a search to find all the boats. Further, they are conversation pieces.
- As the artist incorporates details directly during the casting process, the precise forms he chooses will not be known until the sculptures are fabricated.

Permanent Integrated Commission

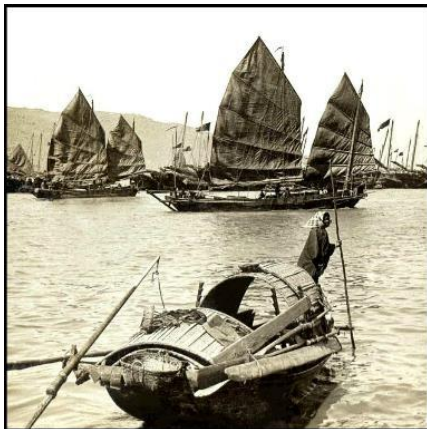
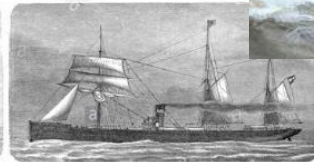
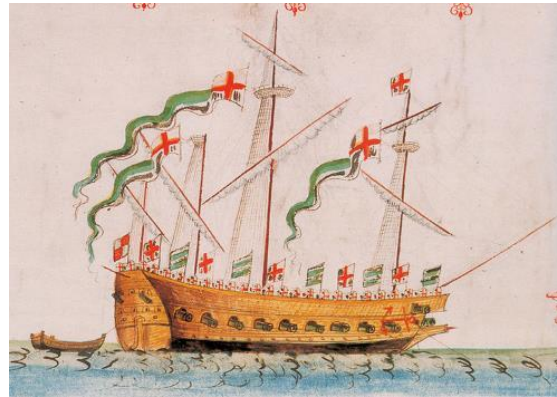
Methodology and Community Involvement

- To develop the design of the sculptures Hew has gathered information on the appropriate heritage of the boats, their decoration and symbolism. In many cultures ships are symbolic of a persons' passage through life and as a container of the soul. Histories will be suggested by specific decorative details and cargos which has been the focus of his consultation.
- Hew has undertaken significant research with guilds and companies (Watermen & Lightermen, Hudson Co, East India Company, Thames Iron Works), history from the Elizabethan period – including the various waves of immigration and different diaspora groups in the area. Further research includes visits to St Matthias Church (built by the East India Co.), St John's Churchyard Wapping and its sailors' gravestones, and St Paul's Church Shadwell where Captain Cook worshipped.
- Hew has met with and consulted: the Maritime Museum and Museum of London; the Boitok Group (St Hilda's East Community Centre), the Coriander Club (Spitalfields City Farm) and the Chinese Association. Future consultation is planned with the Hermitage Community Moorings and Tarling East Community Centre.
- The artist presented his proposal to a community drop in event on 23 March 2017 and at a follow up engagement event dedicated to the artworks on 12 December 2017, when residents provided further historic information specific to the KEMP area to inform the detail he will integrate into the sculptures during the casting process

Initial Concept – East London's Shipping Parishes – Gateway to the World



Initial Concept Research



Art Plinth Concepts

The below shows the emerging artistic designs to be on show at the Art Plinths.

